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## CITY PLANS PANEL

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Meeting to be held in Civic Hall, Leeds on  
Thursday, 15th February, 2024  
at 1.30 pm

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### MEMBERSHIP

#### Councillors

C Campbell  
B Anderson  
D Blackburn  
K Brooks  
P Carlill  
D Cohen  
K Dye  
R Finnigan  
C Gruen  
A Khan  
A Maloney  
J McKenna (Chair)

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**Agenda compiled by:**  
**Andy Booth, 0113 3788665**  
**Governance Services,**  
**Civic Hall**

Enquiries specific to planning applications on the agenda should be directed to Panel Team; Phone 0113 3786980  
Email: [planspanel@leeds.gov.uk](mailto:planspanel@leeds.gov.uk)

# A G E N D A

Item No	Ward	Item Not Open		Page No
1			<p><b>APPEALS AGAINST REFUSAL OF INSPECTION OF DOCUMENTS</b></p> <p>To consider any appeals in accordance with Procedure Rule 15.2 of the Access to Information Rules (in the event of an Appeal the press and public will be excluded)</p> <p>(*In accordance with Procedure Rule 15.2, written notice of an appeal must be received by the Head of Governance Services at least 24 hours before the meeting)</p>	
2			<p><b>EXEMPT INFORMATION - POSSIBLE EXCLUSION OF PRESS AND PUBLIC</b></p> <p>1 To highlight reports or appendices which officers have identified as containing exempt information, and where officers consider that the public interest in maintaining the exemption outweighs the public interest in disclosing the information, for the reasons outlined in the report.</p> <p>2 To consider whether or not to accept the officers recommendation in respect of the above information.</p> <p>3 If so, to formally pass the following resolution:-</p> <p><b>RESOLVED</b> – That the press and public be excluded from the meeting during consideration of the following parts of the agenda designated as containing exempt information on the grounds that it is likely, in view of the nature of the business to be transacted or the nature of the proceedings, that if members of the press and public were present there would be disclosure to them of exempt information, as follows:-</p>	

Item No	Ward	Item Not Open		Page No
3			<p><b>LATE ITEMS</b></p> <p>To identify items which have been admitted to the agenda by the Chair for consideration</p> <p>(The special circumstances shall be specified in the minutes)</p>	
4			<p><b>DECLARATION OF INTERESTS</b></p> <p>To disclose or draw attention to any interests in accordance with Leeds City Council's 'Councillor Code of Conduct'.</p>	
5			<p><b>APOLOGIES FOR ABSENCE</b></p>	
6			<p><b>MINUTES - 18 JANUARY 2024</b></p> <p>To confirm as a correct record, the minutes of the meeting held on 18 January 2024</p>	7 - 10
7	Hunslet and Riverside		<p><b>APPLICATIONS 23/01249/FU AND 23/01250/LI - THE ROUNDHOUSE, WELLINGTON ROAD, LS12 1DR</b></p> <p>To receive and consider the report of the Chief Planning Officer for applications for the change of use and conversion of the existing Roundhouse to provide indoor and outdoor leisure facilities for temporary use inclusive of enclosed Padel courts, changing/welfare facilities and external parking.</p>	11 - 44
8	Hunslet and Riverside		<p><b>APPLICATION 23/06266/FU - SITE TO THE NORTH OF WHITEHALL ROAD (LAND AT THE FORMER DONCASTER MONK BRIDGE WORKS)</b></p> <p>To receive and consider the attached report of the Chief Planning Officer regarding an application for the construction of an office building with associated facilities, parking and landscaping.</p>	45 - 74

Item No	Ward	Item Not Open		Page No
9	Hunslet and Riverside		<p><b>APPLICATION 22/04852/FU - HOLDFORTH COURT, BRUSSELS STREET, LEEDS, LS9 8AT</b></p> <p>To receive and consider the attached report of the Chief Planning Officer regarding an application for proposed demolition works and erection of 10 storey building to create co-living residential development.</p>	75 - 120
10			<p><b>DATE AND TIME OF NEXT MEETING</b></p> <p>Thursday, 14 March 2024 at 1.30 p.m.</p>	

**Third Party Recording**

Recording of this meeting is allowed to enable those not present to see or hear the proceedings either as they take place (or later) and to enable the reporting of those proceedings. A copy of the recording protocol is available from the contacts named on the front of this agenda.

Use of Recordings by Third Parties– code of practice

- a) Any published recording should be accompanied by a statement of when and where the recording was made, the context of the discussion that took place, and a clear identification of the main speakers and their role or title.
- b) Those making recordings must not edit the recording in a way that could lead to misinterpretation or misrepresentation of the proceedings or comments made by attendees. In particular there should be no internal editing of published extracts; recordings may start at any point and end at any point but the material between those points must be complete.

**Planning Services**

To all Members of City Plans Panel

Ninth Floor East  
Merrion House  
110 Merrion Centre  
Leeds LS2 8BB

Contact: Daljit Singh  
Tel: 0113 3787971  
daljit.singh@leeds.gov.uk

Our ref: City Site Visits  
Date: 2.02.2024

Dear Councillor

**SITE VISITS – CITY PLANS PANEL – Thursday 15<sup>th</sup> February 2024**

It has been agreed with the Chair of City Plans Panel to undertake site visits on the morning of the next City Plans Panel meeting as detailed below. The first two site visits also provide an opportunity to visit the new “high line” public realm that has been provided at the former Doncaster Monkbridge site.

A 16 seater minibus has been booked for the site visits. To check numbers please can Members contact [PlansPanel@leeds.gov.uk](mailto:PlansPanel@leeds.gov.uk) as soon as possible if they wish to travel via the minibus.

For those travelling by mini-bus please meet in the **Ante-Chamber, Civic Hall at 9.55am for a prompt start at 10.00am.**

Time	Ward	Site
10.00 am	MINIBUS DEPARTS FROM OUTSIDE CIVIC HALL ENTRANCE	
10.10-10.40	Hunslet & Riverside	Application references 23/01249/FU and 23/01250/LI – Proposed change of use to provide Padel courts at The Roundhouse, Wellington Road, Leeds LS12 1DR
10.50-11.20	Hunslet & Riverside	Application reference 23/06266/FU – Proposed office development at Whitehall Road, Leeds LS12 1BE
11.30-12pm	Hunslet & Riverside	Application reference 22/04852/FU – Proposed co-living residential development at Holdforth Court, Brussels Street, Leeds LS9 8AT

Please notify [PlansPanel@leeds.gov.uk](mailto:PlansPanel@leeds.gov.uk) if you will be attending.

Yours sincerely

Daljit Singh  
Group Manager  
Planning Services

## CITY PLANS PANEL

THURSDAY, 18TH JANUARY, 2024

**PRESENT:** Councillor J McKenna in the Chair

Councillors C Campbell, B Anderson,  
D Blackburn, K Brooks, P Carlill, D Cohen,  
K Dye, C Gruen, A Khan and A Maloney

### 51 Appeals Against Refusal of Inspection of Documents

There were no appeals.

### 52 Exempt Information - Possible Exclusion of Press and Public

There was no exempt information.

### 53 Late Items

There were no late items.

### 54 Declaration of Interests

There were no declarations. In relation to Agenda Item 7, Application 23/0725/FU, Councillor Khan informed the Panel that the site fell within his ward but he would be treating the application with an open mind.

### 55 Minutes - 30 November 2023

**RESOLVED** – That the minutes of the meeting held on 30 November 2023 be confirmed as a correct record.

### 56 Application 23-02725-FU - Site on the North East Side of Cross Green Lane to the west of the junction with Easy Road

The report of the Chief Planning Officer presented an application for the demolition of existing buildings and the erection of 38 apartments and office space. This will represent a phased development to deliver 28 apartments as part of the western phase and 10 apartments with office space as part of the eastern phase. Associated landscaping and car parking was also sought.

The application was deferred at the meeting held in November 2023 to get further information on materials and design. Members visited the site prior to that meeting.

Site plans and photographs were displayed and referred to throughout the discussion of the application.

Draft minutes to be approved at the meeting  
to be held on Thursday, 15th February, 2024

Further issues highlighted in relation to the application included the following:

- Improved elevation images and CGI images had been submitted.
- The images displayed a variety of elevation treatments, including balconies and the solar shading to be used on the south facing elevation.
- The roof would include green roof areas, solar panels and a walled garden.
- The building would be finished in blue/grey brick with elements of off-white render which would help break up the mass of the building.
- The proposals would not affect the functionality of the industrial units to the rear.
- The rear elevation would also be blue/grey brick with off-white render. There would also be semi-transparent polycarbonate walling to the stairwells.
- All window frames, door frames and sills would be finished in a dark grey coated aluminium.
- Samples of materials to be used were made available for inspection.
- The solar shading would also provide winter gain with the capture of sunlight.
- The apartments would have a high amenity level with sustainable climate control.
- The green roofs and walled roof garden would add to the biodiversity improvements of the development.

In response to questions from the Panel, discussion included the following:

- There would be a loading bay to the front of the building and double yellow lines would be extended to the access to the site. There would also be additional assessments for future controls with regards to parking.
- There would not be any main living spaces with windows facing on to the rear of the site.
- There would be a footpath that ran round the rear of the building to provide emergency access/exit to the western end of the building. This had been approved by Health and Safety.

In response to comments from the Panel, discussion included the following:

- The images had provided a much clearer view of the proposals and the design, materials and layout were good.
- The possibility of having a competition or involvement of local schools in designing artwork for the solar shading and for Panel to visit when it is delivered on site.
- The design was good and would bring interest to the streetscene. The proposed colour scheme was notable.
- There was some concern that the second phase of the development would not be carried out or different proposals could be sought for the



second stage. It was reported that the desire to complete the scheme in two phases was to give flexibility but the intention was to develop the scheme as a whole. Should there be any differences these would be subject to planning permission.

- Members were supportive of the scheme.

**RESOLVED** – That approval be deferred and delegated to the Chief Planning Officer subject to the specified conditions set out in Appendix 1 (and any others which he might consider appropriate) and a Section 106 agreement to include the following obligations:

- Affordable housing 7% - 3 apartments comprising two-bedroom flats numbered 3, 20 and 25 on the first, second and third floor plans will be offered to local housing associations for purchase at the council's affordable benchmark prices.
- Contribution towards green space improvements of £49,360.85
- Traffic Regulation Order to manage any overspill parking issues in the surrounding area as a result of the development: £10,000.
- Co-operation with Local Jobs and Skill Initiatives.

In the circumstances where the Section 106 has not been completed within 3 months of the Panel resolution to grant planning permission, the final determination of the application shall be delegated to the Chief Planning Officer.

**57 Date and Time of Next Meeting**

Thursday, 15 February 2024 at 1.30 p.m.

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**Leeds**  
CITY COUNCIL

Originator: R Coghlan  
Telephone: 0113 336 3775

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## Report of the Chief Planning Officer

### *City Plans Panel*

Date: 15<sup>th</sup> February 2024

**Subject: 23/01249/FU and 23/01250/LI Change of Use and conversion of the existing Roundhouse to provide indoor and outdoor leisure facilities for temporary use inclusive of enclosed Padel courts, changing / welfare facilities and external parking at The Roundhouse, Wellington Road, Leeds, LS12 1DR.**

**Applicant: UK Padel Club Ltd**

#### **Electoral Wards Affected:**

Hunslet and Riverside

No

Ward Members consulted

#### **Specific Implications For:**

Equality and Diversity

Community Cohesion

Narrowing the Gap

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**RECOMMENDATION: DEFER and DELEGATE to the Chief Planning Officer for approval subject to the specified conditions set out in Appendices 1 and 2 (and any others which he might consider appropriate) and the completion of a Section 106 agreement to include the following obligations:**

- Travel Plan Review fee of £3,682
- Transfer of land for A58 footbridge improvement
- Monitoring fee

## **1. Introduction:**

- 1.1. These applications are brought before City Plans Panel under Part 1(b) of the Exceptions to the Officer Delegation Scheme. Part 1(b) states “the determination of applications for development that would constitute a significant departure from the Development Plan, including a significant departure from any Local Development Framework currently in force.” In this case, the external area of the site, to the east of the Roundhouse building, form part of the Housing Allocation HG2-113 of the Site Allocations Plan, and the proposal is for a non-housing use.
- 1.2. This is a joint report for both the listed building and planning applications. Members of City Plans Panel are asked to determine the principle of the proposed temporary use of the site for padel courts contrary to the allocation of the external areas of the site for housing. The case for permitting the padel courts use in principle is set out in the appraisal (Section 10 below). It is considered that all other planning matters are either resolved now, are dealt with by condition / S106 Agreement or will be resolved under delegated powers. Where outstanding points are awaiting agreement, a note of this is made in the report.

## **2. Proposal**

- 2.1. The applicant “UK Padel Club Ltd” would like to open a padel sports facility on the site. Padel sport is a cross between tennis and squash using courts of approximately 10.3m x 20.3m size. The proposed development is to change the use of the property from vehicle hire / office use to indoor and outdoor leisure facilities.
- 2.2. The roundhouse building would provide 3,096sqm of indoor sport and recreation use (Class E (d) [of schedule 2 of the Town and Country Planning \(Use Classes\) Order 1987](#)) including 5 padel courts, changing rooms and café / retail area (390sqm). The external areas of 5,641sqm would provide 5 courts, car parking, cycle parking and landscaping.
- 2.3. The proposed alterations to the Roundhouse listed building are relatively minor involving the stripping out of modern partitions, the siting of five courts around the perimeter, each with a 4m high metal frame enclosure, and the formation of changing facilities, toilets and café/retail space within the central colonnade.
- 2.4. The proposed use is temporary for 10 years from the commencement of the use. After that time, all external court infrastructure will be removed. This will release the external land for housing development in accordance with the housing allocation

## **3. Site and Surroundings:**

- 3.1. The site itself includes the historic roundhouse building at the front of the site next to Wellington Road and a surface car park to the east. The roundhouse

was built as a circular storage building for steam locomotives around a central turntable. Constructed of redbrick with paired round-headed windows and a pitched slate roof with rooflights, it was completed in 1847 and is grade II\* listed.

- 3.2. The application when first submitted also included a plot of vacant scrub land of 4,590sqm that stretched eastwards towards the canal. The vacant scrubland has now been removed from the red-line boundary of the applications. This land is owned by the same landowner – Wellbridge Properties Ltd – and has independent access off Graingers Way, so remains available for housing development.
- 3.3. The site is situated to the south of Graingers Way, and to the north of the Galliford Try Monkbridge Whitehall Road development of blocks of flats to the south. The boundary to Graingers Way comprises iron railings and brick piers with two gated entrance points for vehicles. The land close to the roundhouse is mainly tarmac with a raised linear platform running along the southern boundary.
- 3.4. The land comprising the outdoor car park and the vacant scrub land (not part of the site) is allocated for housing development in the Site Allocations Plan under reference HG2-113. The allocation is part of the Inner Areas housing characteristic area with an indicative capacity for 310 dwellings. Site requirements include an expectation for a financial contribution toward highway mitigation measures relating to A65 / A58 gyratories and a need to preserve the special architectural or historic interest of Listed Buildings and their setting.
- 3.5. The site is within the city centre as defined in the UDP and Core Strategy.
- 3.6. On the north side of Graingers Way is the half roundhouse and terrace of engine repair shops which formed part of the original railway infrastructure built at the same time as the roundhouse. These are grade II listed.

#### **4. Relevant Planning History**

- 4.1. 20/04640/LI and 20/04639/FU Planning and Listed Building applications for replacement of rooflights. Approved 21/10/20.
- 4.2. 20/623/96/LI Listed building application to demolish part and alterations and extension to form business retail and restaurant units. Approved 7/8/97.
- 4.3. 20/281/96/LI Listed building application to carry out alterations to form internal first floor offices. Approved 14/01/97.
- 4.4. 20/438/94/LI Listed building application for 3 internally illuminated signs and flood lights to garage. Refused 12/12/94.

#### **5. History of Negotiations**

- 5.1. The initial applications were for the Roundhouse and the external land stretching eastwards to the Leeds-Liverpool Canal. The external land corresponded with the Housing Land Allocation HG2-113. The applicant subsequently agreed to remove the eastern section of the land (Phase 2) from the applications so it remains available for housing development. Further explanation is provided in the “Principal of Development” section of the appraisal below.
- 5.2. The applicant also agreed to the removal of a proposed padel court to the forecourt area in front of the Roundhouse building facing Wellington Road.

## **6. Public/Local Response**

6.1. Planning and listed building application publicity consisted of:

- i. Leeds City Council Public Access Website posted 5/4/23
- ii. Site Notices posted 14/4/23
- iii. Press Notices posted 28/4/23

6.2. Leeds Civic Trust

6.2.1. We support the re-use of this important listed building and the fact that it will not involve any alterations to the original structure. However, the proposal also provides the opportunity to make positive improvements to the building, such as the restoration of the existing pairs of arches which have been combined to create wide doorways, not needed by this use, and this would be a welcome benefit.

6.2.2. *Response: revised drawings have been submitted to illustrate restoration of the existing pairs of arches.*

6.2.3. On the other hand, we strongly object to the single external padel court to the south-west of the building. This is shown on the layout plan but not referred to in the Design and Access Statement and its enclosure would adversely affect the setting of the listed building.

6.2.4. *Response: this padel court has been removed from the scheme.*

6.2.5. With respect to the external layout, one question is whether there is sufficient parking, particularly if phase 2 is developed? While there could be pedestrian access from the adjoining Junction development (this should be confirmed), and the pedestrian bridge over Wellington Road, which might enable some users to walk, the use will still generate significant car travel.

6.2.6. *Response: the land for phase 2 has been removed from the scheme. The applicants have agreed to assist with pedestrian and cycle path improvements including access to the pedestrian bridge. See the Sustainable Transport and Travel Planning section of the appraisal below for more explanation.*

6.2.7. There is no indication of any landscape proposals - these should form part of the application. The layout leaves little space for this and as a result tends toward the over-intensive. The cycle parking, while well-situated for the

external courts, is not in a convenient position for those using the internal courts. There is adequate space in the angles between those courts for more convenient cycle parking. The Design and Access Statement refers to the desire to cover the external courts with a tensile roof. There are no details of this so we are unable to comment on the possible effect on the setting of the listed building and presumably any permission would not include such roofs:

- 6.2.8. *Response: landscaping proposals have been provided – see Design, Landscape and Biodiversity section of the appraisal below. Long stay cycle parking has now been included within the Roundhouse building. It can be confirmed that the external padel courts will not have roofs.*

## 7. Consultation Response

### Statutory

#### Canal and River Trust

- 7.1. The submitted plans do not fully show the extent of changes proposed to the landscaping to the northeast of the site close to the canal. Changes to boundary treatments, planting or new external structures could be visible from the canal and have the potential to impact upon the wider setting of the waterway corridor. We therefore request that hard and soft landscaping details should be provided prior to the development of the external areas including details of any boundary treatments. Details could be reserved through the use of appropriately worded conditions.
- 7.2. In addition, it is not clear if any new structures are proposed alongside the new external sports facilities that could impose loading onto the cutting with the canal, which could result in an increased risk of land instability next to the waterway. The Trust is also concerned about the impact of external lighting could disturb wildlife associated with the canal corridor, including bat populations that may forage over the water at night. Lighting details could be reserved through the use of appropriately worded conditions. The Trust also recommends applying their standard informative providing contact information.
- 7.3. *Response: the removal of land that abuts the canal from the red-line boundary of this scheme significantly reduces the potential impact of the proposal on the canal. There will be no structures close to the canal that could cause land instability. Nevertheless, landscaping and lighting proposals have been considered [see appraisal section below] and appropriate conditions and informatives are recommended.*

#### Yorkshire Water

- 7.4. No objection subject to applying standard conditions. In terms of water supply a new supply for this development can be made available, subject to demand, from the 100mm Ductile Iron main in Grainger's Way. Any other suggested connection routes would be extremely problematic.

#### Historic England

- 7.5. Historic England welcomes the proposals for reuse of the Grade II\* listed Roundhouse. The use as a leisure facility with Padel courts located around the perimeter of the interior of the building is a very interesting new use for the building and it appears it would have a minimal physical impact on the building. There is a slight lack of detail on some elements of the scheme and we recommend further information is submitted for consideration by your authority's specialist Conservation Officers. In particular any physical interventions needed to accommodate the new changing, toilet and restaurant facilities in the centre of the Roundhouse. Given the historic use of the building, there could be archaeological remains below the existing floor, such as old railway tracks or pits. Care should be taken that any excavations do not damage these features.
- 7.6. There is considerable scope for enhancing the external appearance and setting of the listed building. Again, there is a lack of detail in this regard. We recommend consideration is given to improving the landscaping to the west side of the building, facing onto Wellington Road. We recommend the Padel court shown in this location is omitted from the scheme and replaced with suitable landscaping and boundary treatments.
- 7.7. Historic England recommends that its concerns outlined above be addressed in order to meet the requirements of the NPPF and statutory duties of sections 16(2) and 66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 to have special regard to the desirability of preserving listed buildings or their setting or any features of special architectural or historic interest which they possess.
- 7.8. *Response: The court proposed to the north west side of the Roundhouse building facing Wellington Road has been removed. More detail of the internal alterations has been submitted and landscaping improvements have been made. Further explanation is provided in the Appraisal section below. A condition is recommended to avoid damage to any archaeological remains below ground as per the recommendation of the West Yorkshire Archaeological Advisory Service.*

#### Historic Buildings and Places

- 7.9. We have no objection to the principle of the change of use and note the application largely involves removal of modern partitions and that the installation of the padel courts only require minimal physical alterations, allowing the full volume of the Round House to be appreciated. However, no details are provided for the proposed toilet and café block to be installed in the central area and this has potential to harm the overall significance. As issues such as setbacks from the engine doorways are important, these details need to be secure now, and not by condition.
- 7.10. *Response: as per the response above to Historic England more detail of the internal alterations have been submitted (see below).*

#### Non-Statutory



## LCC Conservation

- 7.11. (Comments on the original submission) - The proposed use will introduce a new use for this derelict grade II\* listed (building) which will positively exploit its spatial qualities and remove modern partitions which are harmful. However, further information is required to provide a definitive response: - typical proposed sections showing the proposed welfare facilities within the inner courtyard. - elevations showing typical partitions within the inner colonnade. The impact of the proposed external paddle courts is neutral apart from the proposed court adjacent to Wellington Court which is harmful to the setting of the listed building and should be replaced with a sensitive planting scheme.
- 7.12. *In response the proposed court to the front of the Roundhouse has been removed from the scheme and replaced by landscaping. Internal sections show that the changing rooms, toilets and communal areas will not be enclosed by ceilings thereby keeping the existing structure and proposed services exposed. Services will be hung from the existing steel frame. Partition walls will be built up to the underside of existing beams. The existing structure and central lantern will remain exposed.*

## West Yorkshire Archaeological Advisory Service

- 7.13. The roundhouse is located in the former railway engineering yard, Wellington Road, Leeds – the maintenance yard of the Leeds and Thirsk Railway. The yards were established in 1847 on a greenfield site. The earliest surviving structure on the site is the full roundhouse that is the subject of the application. The earliest OS map, 1847, of the site shows two long rectangular sheds to the east of the roundhouse, the northern one survives while the southern one has been demolished. The southern shed is located in the footprint of the proposed development site in the area for external courts and car parking. This shed is marked as a wagon shed on the 1893 OS map. The cartographic evidence does (not) confirm if this building was a wagon shed from its origin or what work was undertaken in the shed, construction or maintenance of wagons, or a combination of both. It is also not known if any of this building survives below ground. As this building dates from the origins of the railway yard it would have been integral to the operation and functions of the railway yard.
- 7.14. The proposed works to the round house will primarily involve alterations and changes to later additions to the roundhouse and a Watching brief (EWY5116) carried out in Jan 1994 during refurbishment of roundhouse. The impact of these works should be minimal.
- 7.15. There is no record of previous archaeological works having been undertaken on the wagon shed and there is the potential for the survival of remains related to this to survive buried on the site. The impact of the proposed works in this area are uncertain as the depth of disturbance and the depth at which buried archaeology could survive are unknown. There is therefore the potential for archaeological remains to survive in this area.

- 7.16. Recommended Course of Action. An appropriate response considering the archaeological potential of the site, its significance and the likely impact of the proposed works would be to undertake a watching brief on groundworks undertaken during construction works on the site. The watching brief should observe and record any remains of the former wagon shed and any buried remains that relate to its use and operation. Should planning approval be granted a condition should be placed on the approval for a programme of mitigation archaeological works to investigate and record any archaeological remains disturbed by the works. The archaeological mitigation works should include an archaeological watching brief on the ground works on the footprint of the wagon shed.
- 7.17. *Response: an appropriate condition is recommended to ensure that the construction works on the footprint of the former wagon shed allow for investigation and recording of any archaeological remains disturbed by the works.*

#### LCC Design

- 7.18. The 'Padel Courts' to the rear. We understand that this 'Padel' is a new variation on an amalgamation of tennis and squash. The courts themselves are quite big and shielded with a mesh etc. They will not be visually attractive as pieces in the townscape. However, the majority are set to the rear of the Roundhouse and could be softened with planting and the like so this might not be an issue. The main issue is the padel court to the front adjacent to the A58 Wellington Road (The Inner Ring Road) an 8 lane main arterial highway. (4 lanes in either direction). We think there are two issues here. Firstly, the safety issue and then the visual amenity.
- 7.19. The visual amenity as setting to a listed building. This location is not suitable for a tennis / squash court in terms of townscape setting for an important building such as this. The courts themselves are not attractive and are not suitable for this location. Overall, as the application is, we would recommend a simple refusal. However, if the applicants are minded to remove the padel court adjacent to the A58 then we feel the application becomes something quite different that might be able to progress subject to the heritage and landscape issues etc.
- 7.20. *Response: the court adjacent to the A58 has been removed from the scheme.*

#### LCC Landscape

- 7.21. Comments of 27/4/23 related to the initial scheme including land up to the canal (Phase 2 for 8 courts) which has now been removed from the scheme. The Landscape Officer raised concerns about the impact on trees and habitat around the edges of this area and asked for a revised layout to minimise the loss of habitat.
- 7.22. Comments of 5/2/24: The site (red line boundary) includes existing landscape amenity planting bed to the northern boundary with the Roundhouse, which should be retained and enhanced if required. The proposed new planting beds

to the Wellington Road frontage, and planting to screen the proposals from overlooking residential buildings to the north and south, are welcomed. However, plans must annotate the type of planting proposed (trees, hedge, shrubs etc) and confirm that soft landscaping proposals will accord with URBAN TREE PLANTING GUIDANCE and the LEEDS PLANTING CHECKLIST. Regarding external hardwork proposals, plans should be annotated to confirm the location and type of any proposed walls, fencing, gates, steps/ramps, the types of paved surfacing (tarmac, flags, sets etc.) street furniture, bin/cycle stores etc.

- 7.23. Response: the drawings showing new landscaping to the Wellington Road frontage, along the length of the raised car parking area, to the eastern boundary of the site and to the Graingers Way frontage have been annotated to set out the broad types of soft landscaping proposed. Similarly, details of the hard landscaping features and areas have also been annotated.

LCC Access Officer

- 7.24. Check where accessible parking will be made available and also accessible WC and changing facilities.
- 7.25. *Response: Four disabled parking spaces will be provided in front of the main entrance, accessible from Graingers Way. A condition is recommended to ensure that these facilities will be accessible to all*

LCC Flood Risk Management

- 7.26. The hydraulic calculations report flooding for the 1% AEP (1 in 100) event. This requires further detail as required under clause 9.7(d) of the Minimum Development Control Standards for flood risk. The appropriate Climate Change Allowance has not been included in the hydraulic calculations. Exceedance flow routing has not been provided. Surface water connectivity has not been proved or approved. Yorkshire Water have previously commented thus:

- Surface water discharges to the public sewer must have a minimum of 30% reduction based on the existing peak discharge rate during a 1 in 1 year storm event.
- The developer will be required to provide evidence of existing positive drainage to a public sewer from the site to the satisfaction of Yorkshire Water and the Local Planning Authority by means of physical investigation. On-site attenuation, taking into account climate change, will be required before any discharge to the public sewer network is permitted.

- 7.27. *Response: the Flood Risk Officer has confirmed that the drainage arrangements are resolvable. Additional information has been commissioned by the applicant. It is recommended that this matter is delegated to the Chief Planning Officer and appropriate conditions are applied.*

LCC Nature Conservation

- 7.28. Biodiversity Net Gain. T Additional landscape works shown on Proposed Master Plan 3950-811 indicate the scheme will achieve an acceptable level of BNG
- 7.29. Impact on the Leeds Habitat Network. The revised development boundary does not include the scrub habitat adjacent to the Leeds Liverpool Canal (named Phase 2) and will not impact on the Leeds Habitat Network.
- 7.30. Bat Roosts. The revised Preliminary Ecological Appraisal Report (PEAR) confirms that one structure (The Roundhouse) has potential roost features and remains within the development boundary. . The Bat Activity Report recommends that bat boxes are installed to suitable structures to provide roosting opportunities. Should the application be approved, such a scheme to provide biodiversity enhancements for species can be secured through condition.
- 7.31. Scheme Lighting and its impacts on bats. The scheme includes 6m lighting columns over the external padel courts, that provide over 40,000 lumens each. Spill from these floodlights has the potential to increase artificial light levels on bat foraging and commuting habitat to the west of the site – identified in the Bat Activity Report as areas of moderate levels of bat activity. To avoid impacts on foraging and commuting bats through the introduction of additional lighting to this area, Nature Team recommend that should the scheme be approved, a condition is attached requiring the production of a Lighting Design Strategy for Bats.
- 7.32. Invasive Non-Native Species (INNS). The PEAR notes Schedule 9 species, Cotoneaster, is present on site. It also acknowledged that the survey for these was constrained by areas of the site being inaccessible. A method statement for the control and eradication of INNS can be secured through planning condition.
- 7.33. *Response: it has been clarified that appropriate bat emergence surveys have been carried out. Conditions are recommended to ensure installation of bat boxes, to ensure that the external lighting will be designed to avoid unacceptable impacts on bats and to require a method statement to ensure the eradication of the cotoneaster INNS.*

LCC Environmental Health (Pollution Control)

- 7.34. Given the newness of padel as a sport in England it is acknowledged that noise survey information is not widely available. Therefore having considered the location of the application in relation to existing residential uses and the current likely prevailing background sound levels due primarily to road traffic in the area, we consider that a pragmatic approach in lieu of a noise assessment would be for a restriction of hours of play. This is to provide respite for residents in the area who may be impacted upon from sport noise and other noise associated with the development such as patrons comings and goings, deliveries/waste collections and fixed plant. Fixed plant could be dealt with via our standard conditions and a restriction on hours of use such as limiting weekday and Saturday use until 9:00 PM during the week and 6:00 PM on

Sunday. Once established, operational noise could later be assessed in the context of extending these hours as per the original application based on the findings of the noise assessment via a section 73 application. Recommendation- Approval with conditions.

#### LCC Environmental Studies (Transport Noise and Air Quality)

- 7.35. We have no objection to this proposed development in terms of transportation noise. Environmental health should be consulted with regard to any other potential noise issue.

#### LCC Highways

- 7.36. Revised plans are required addressing the following:
- Red line boundary shall be amended.
  - The proposed shared footway/cycleway on Graingers Way should be widened to 3m where possible.
  - Location of bin stores and refuse collection strategy should be provided.
  - Pedestrian walkway adjacent to CP-5 up to CP-14 should be moved to the northern side.
  - The internal road width adjacent to CP-14 should at least be 3.3m wide.
  - Details of the proposed long stay cycle parking and motorcycle parking shall be provided.
  - Off site highway works plan shall be amended in accordance with the comments under 'off site highway works. These include a new cycle and pedestrian path to Wellington Road and Graingers Way and land dedicated to the City Council to facilitate the footbridge improvement over Wellington Road.
- 7.37. *Response: These matters are addressed in the Sustainable Transport section of the appraisal below.*

#### LCC Influencing Travel Behaviour

- 7.38. Comments of 27/10/23. The submitted travel plan does not meet the requirements of the Transport SPD, Part 4, Travel Plans. Table 4-28 of the SPD includes a Travel Plan checklist, for ease of reference. The travel plan needs to be amended to meet the requirements of the SPD.
- 7.39. The travel plan should set out an annual budget for the travel plan co-ordinator to use to fund incentives. Although the budget given to the TPC has been mentioned in the new Travel Plan, the term "(to be confirmed in due course)" will not suffice, an amount should be listed, this must also include when the TPC will be given the budget to help promote sustainable travel options via the methods noted in 7.14, for example at occupation or annually for 5 years after the travel plan has commenced.
- 7.40. A Travel Plan Review fee of £3682 will be required.
- 7.41. *Response: the applicant has agreed to pay the £3,682 review fee and to revise the Travel Plan as recommended.*

## LCC Local Plans

- 7.42. Local Plan officers concluded that they will accept the proposal in principle subject to confirmation of the following in writing:
- i. That the applicant would accept a temporary planning permission of 10 years for Padel Court use of Phase 1, with a condition that all court infrastructure shall then be removed. We would also need to have the description of development changed to include the word temporary.
  - ii. That the Phase 2 part of the site will be excluded from the padel court site / scheme, so that it remains available for housing development
  - iii. That the landowner has no current interest in developing the entire site for housing.
- 7.43. These conditions have been satisfied. The landowner, Wellbridge Properties Ltd has confirmed that, regarding the land for the padel court use, it has no intention to sell the land or develop the land for residential purposes for the next 10 years. The land shown for Phase 2 has been removed from the applications and remains available for housing development with independent access. The application is now for temporary 10 year period, after which time the outdoor court infrastructure will be removed.

## Contaminated Land Team

- 7.44. The site has been used for engineering purposes for the nearly the full duration of its existence so the land will be contaminated. It has been confirmed that the ground surface will be tarmac, that no soil will be imported and that active mechanical ventilation compliant with regulations and British Standards is proposed. However, the new soft landscaping may involve some breaking through the hard surfaces and generate a need for further assessment work.
- 7.45. Depending on whether the proposed landscaping would be in raised planters with no breaking of ground, or set into the ground, a different set of conditions would be required. If all landscaping requires no breaking of ground conditions dealing with unexpected contamination and imported soil would be required. If the landscaping would involve breaking of ground, a Phase 1 Desk Study would be required, and subsequently possibly a Phase 2 (site investigation) report and/or a Remediation Statement.
- 7.46. *Response: Standard conditions are recommended to secure necessary study / investigations.*

## 8. Relevant Planning Policies

### 8.1. Statutory Context

- 8.1.1. Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires the application to be determined in accordance with the development plan unless material considerations indicate otherwise. For the purposes of decision making at this site, the Development Plan for Leeds currently comprises the following documents:
- The Leeds Core Strategy (Adopted November 2014)

- Saved Leeds Unitary Development Plan Policies (2006), included as Appendix 1 of the Core Strategy
- The Natural Resources & Waste Local Plan (NRWLP, Adopted January 2013) including revised policies Minerals 13 and 14 (Adopted September 2015).
- Aire Valley Leeds Area Action Plan (AVLAAP, Adopted 2017)
- Site Allocation Plan 2024

8.1.2. These development plan policies are supplemented by supplementary planning guidance and documents.

8.1.3. Section 66 of the (the “Listed Building Act 1990”) reads:

“In considering whether to grant planning permission... for a development which affects a listed building or its setting, the local planning authority...shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses”

8.1.4. Section 16(2) of the same act reads:

“In considering whether to grant listed building consent for any works the local planning authority or the Secretary of State shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.”

## **Development Plan**

### **8.2. Leeds Core Strategy (CS)**

8.2.1. The adopted CS sets out strategic level policies and vision to guide the delivery of development investment decisions and the overall future of the district. The most relevant policies are set out in the paragraphs below:

Objective 14: Support the improved public health and wellbeing of Leeds’ residents and workforce.

Spatial Policy 1: Location of Development prioritises the redevelopment of previously developed land within the Main Urban Area, prioritising urban regeneration and taking advantage of existing services and high levels of accessibility.

Spatial Policy 3: Role of Leeds City Centre. i) Promoting the City Centre’s role as the regional capital for major new retail, leisure, hotel, culture and office development.

Policy CC1 other town centre uses will be supported within the City Centre boundary provided the use does not negatively impact on the amenity of neighbouring uses.

Policy P9 states that access to local community facilities and services, including health, sport and recreation facilities, is important. Such facilities should be accessible by foot, cycling, or by public transport and should not adversely impact on residential amenity and should where possible, and appropriate, be located in centres with other community uses. The scale of the facility or service should be considered in conjunction with the level of need within the community and its proposed location within the Settlement Hierarchy.

Policy P10: Design requires new development to be based on a thorough contextual analysis to provide good design appropriate to its scale and function. New development is also required to deliver high quality inclusive design. Policy P10 sets out a series of key design principles (i to vi) for new development, in relation to size, design, layout, existing assets, amenity and accessibility.

Policy P11: Heritage states that the historic environment and its settings will be conserved, particularly those elements which help to give Leeds its distinct identity.

Policy P12: states that landscapes will be conserved and enhanced.

Policies T1: Transport Management and T2: Accessibility Requirements and New Development identify transport management measures and accessibility measures to ensure new development is adequately served by highways and public transport, and provides safe and secure access for pedestrians, cyclists and people with impaired mobility.

Policy G9: states that development will need to demonstrate biodiversity improvements.

Policy EN5 – flood risk. A flood risk assessment including an evacuation plan and sequential test/exceptions tests would be required as the site lies in Flood Zone 2.

Policy EN6 – Waste Management. Development is expected to demonstrate measures to reduce and re-use waste both during construction and throughout its life and to provide sufficient space to enable separation, storage and collection of recyclable materials.

Policy EN8 identifies electric vehicle charging infrastructure requirements.

Policy ID2 outlines the Council's approach to planning obligations and developer contributions.

### **8.3. Leeds Unitary Development Plan Review 2006 (UDPR) Saved Policies**

#### **8.3.1. Relevant Saved Policies include:**

Policy GP5 all planning considerations

Policy BD2 design and siting of new buildings

Policy BD4 mechanical plant

Policy BD5 residential amenity



Policy LD1 landscaping

**8.4. Leeds Natural Resources and Waste DPD 2013 Part / 2015 Part**

8.4.1. The Natural Resources and Waste Local Plan was adopted by Leeds City Council on 16th January 2013. The Natural Resources and Waste Development Plan Document (Local Plan) is part of the Local Development Framework. The plan sets out where land is needed to enable the City to manage resources, like minerals, energy, waste and water over the next 15 years, and identifies specific actions which will help use natural resources in a more efficient way.

8.4.2. Relevant policies include:

Air 1 management of air quality through new development

Water 4 development in flood risk areas

Water 6 flood risk assessments

Water 7 surface water run-off

Land 1 contaminated land

Land 2 development and trees

**8.5. Site Allocations Plan 2024**

8.5.1. The land to the east of the roundhouse, south of Graingers Way is allocated for housing development under Policy HG2, reference 113. It has an indicative capacity of 310 dwellings. Site requirements include an expression of suitability for elderly persons accommodation, an expectation for a financial contribution toward highway mitigation measures relating to A65 / A58 gyratories and a need to preserve the special architectural or historic interest of Listed Buildings and their setting.

**8.6. Relevant Supplementary Planning Guidance:**

SPD Transport

SPD Accessible Leeds

**8.7. National Planning Policy Framework (NPPF) December 2023**

8.7.1. The NPPF sets out the Government's planning policies for England and how these should be applied (para 1), and is a material consideration in planning decisions (para 2). It states that the purpose of the planning system is to contribute to the achievement of sustainable development (para 7). So that sustainable development is pursued in a positive way at the heart of the Framework is a presumption in favour of sustainable development (paras 10-11). It states that decision makers at every level should seek to approve applications for sustainable development where possible (para 38). The Framework sets policies on the following issues which are relevant to this planning application proposal (including section numbers):

2 Achieving sustainable development (paras 7, 8, 10, 11, 12)

4 Decision making (paras 38, 39, 40, 41, 43, 47, 48, 54, 55, 56, 57, 58)

5 Delivering a sufficient supply of homes (60, 63, 64, 66)

8 Promoting healthy and safe communities (96, 97, 102, 104)

9 Promoting sustainable transport (108, 109, 114 - 117)

11 Making effective use of land (123 – 128)

12 Achieving well designed and beautiful places (131-141)

14 Meeting the challenge of climate change and flooding (157-175)  
15 Conserving and enhancing the natural environment (180, 186, 189-194)  
16 Conserving and enhancing the historic environment (195, 200, 201, 203, 205 - 213)

- 8.7.2. Paragraph 124 (c) says planning decisions should give substantial weight to the value of using suitable brownfield land within settlements for homes and other identified needs.
- 8.7.3. Paragraph 96 c) says planning decisions should aim to achieve healthy inclusive and safe places which enable and support healthy lifestyles, especially where this would address identified local health and well-being needs – for example through the provision of safe and accessible green infrastructure, sports facilities, local shops, access to healthier food, allotments and layouts that encourage walking and cycling.
- 8.7.4. Paragraph 97 a) says planning decisions should plan positively for the provision of community facilities (including sports venues).
- 8.7.5. Paragraph 102 notes that opportunities for sport and physical activity is important for the health and well-being of communities.

## **9. Main Issues**

- 9.1. Principle of the development
- 9.2. Noise and amenity
- 9.3. Design, Landscape and Biodiversity
- 9.4. Sustainable Transport and Travel Planning

## **10. APPRAISAL**

### **10.1. Principle of the Development**

- 10.1.1. Policy HG2 (113) of the Site Allocations Plan (SAP) allocates the external land to the east of the Roundhouse for housing development, with a capacity of 310 dwellings and site requirements indicating suitability of the site for older persons or independent living, for highway contributions and special consideration to protecting the setting of listed buildings.
- 10.1.2. Policy GP1 of the Unitary Development Plan restricts permanent use of allocated sites to that specified in the allocation, subject to exceptions for i) auxiliary uses, ii) mixed use in which the specified use will be the principal use, or iii) where evidence is demonstrated of changed circumstances of need for the specified use or suitability of the site.
- 10.1.3. A number of policies of the Core Strategy are supportive of recreational, leisure or sporting uses in this location. City Centre Policies SP3 and CC1 are supportive of leisure development, subject to impact on amenity of neighbouring uses. Policy P9 is supportive of community uses, that can

include sport and recreational facilities, subject to impact on neighbouring amenity. Facilities should be accessible by non-car modes and be appropriate in scale to the hierarchy of settlement.

10.1.4. The outdoor element of the proposal needs to be considered against Policy HG2 of the SAP which allocates that land for housing (allocation HG2 113). The Roundhouse itself is not part of that allocation. The scheme initially submitted was considered contrary to this policy. Given the housing crisis, the Council will not want to give up potential housing development sites.

10.1.5. However, the scheme has been revised significantly and now accords with Policy GP1 of the UDP which governs the use of allocated land. The key conditions set by the Local Plans Team have been met as follows:

- i. Making the padel court use temporary for 10 years. On expiry, the applicant is agreeable to removing the courts so that the land becomes available for housing development. Policy GP1 states that permission should not be granted for any *permanent* uses (other than the specified use). It does not provide any guide on length of temporary uses.
- ii. Removing the Phase 2 land from the application. This land of 0.46 ha will remain available for housing development. The area is just under half the size of the housing allocation of 0.99ha. This removal comes close to meeting the mixed use exception of Policy GP1, although housing will not be the principal use.
- iii. Confirmation from the landowner that it has no intention of selling or developing the land for housing during the 10 year period. This was one of the housing allocations in the SAP put forward by the council rather than a landowner/developer, where support for the allocation by the landowner was assumed rather than confirmed. In practice, this means there is a big question mark over whether the land is available for housing development anyway.

10.1.6. So, it may be concluded that the revised scheme is consistent with the policies concerning allocation for housing use.

10.1.7. Regarding the proposed use, there is policy support. The use for padel court sport would be a combination of indoor leisure (class Ed) and outdoor sport (class F2c). Policy SP3 supports major leisure in the city centre and Policy CC1g supports other town centre uses (which includes indoor leisure) in the city centre. There is also general support for facilities that contribute to health and wellbeing in Objective 14 of the Core Strategy. Policy P9 sets criteria for community recreation and sporting facilities; they should be accessible by non-car modes and be of a scale appropriate for the settlement. In this case, the site location in the city centre is very accessible by public transport, by bike and by walking.

10.1.8. Overall it is considered that the principle of the revised temporary development proposals is acceptable, in accordance with Policy GP1 of the UDP and policies SP3, CC1 and P9 of the Core Strategy.

## **10.2. Noise and amenity**

- 10.2.1. Policies CC1 and P9 of the Core Strategy make their support for leisure / sport uses conditional on there being no unacceptable loss of amenity to neighbours. This site has the A58 to the north which is very noisy, but new residential flats to the south and offices on the north side of Graingers Way that could be affected. Because padel boarding is a new sport, the applicant has not been able to source any empirical evidence of typical noise levels likely to be generated. The applicant has applied for hours of operation of 7-30am – 11-30pm. However, given the location and likely noise the Environmental Health Officer recommends that the hours of operation be limited to 8am – 9pm Monday to Saturday and 9am – 6pm Sundays and Bank Holidays.
- 10.2.2. After a period of operation when the level of noise can be measured, the applicant would be free to submit an application to extend the hours supported by evidence of noise impact. Assuming that levels are tolerable for the location and no complaints have been received such an application could be considered favourably.

## **10.3. Design, Landscape and Biodiversity**

- 10.3.1. Policies P10, P11 and P12 of the Core Strategy expect schemes to be appropriately designed for the site context conserving the historic environment and conserving and enhancing landscapes. Section 66 of the Planning (Listed Buildings and Conservation Areas) Act 1990 (the 'Listed Buildings Act 1990') provides:

“In considering whether to grant planning permission for development which affects a listed building or its setting, the local planning authority or, as the case may be, the Secretary of State shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.”

- 10.3.2. Section 16(2) of the same act reads:

“In considering whether to grant listed building consent for any works the local planning authority or the Secretary of State shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.”

- 10.3.3. Policy P11 of the Core Strategy seeks to conserve historic buildings, including the 19<sup>th</sup> century transport network, with development proposals expected to demonstrate a full understanding of historic assets.
- 10.3.4. Further, in accordance with paragraph 205 of the NPPF great weight should be given to the conservation of heritage assets.
- 10.3.5. The Conservation Officer considers that the new use will positively exploit the spatial qualities of the Roundhouse, and remove the harm of modern partitions. The requested detailed sectional and elevational drawings of the internal facilities and partitions have been provided illustrating that the historic integrity

of the roundhouse building will be preserved. The elevations show that the two industrial door openings on the north side of the building will be reinstated as double brick arches, which will be a considerable enhancement.

- 10.3.6. The proposed court to the front of the building has been removed and replaced with landscaping, as recommended by the Conservation Officer and Historic England.
- 10.3.7. Policy G9 expects there to be an overall net gain for biodiversity and that existing wildlife habitats are safeguarded and enhanced. The revised scheme, with additional landscaping provides an acceptable biodiversity net gain.
- 10.3.8. It has been clarified that the bat emergence survey carried out July 2023 is sufficient. The submitted Bat Activity Report recommends bat boxes be installed to suitable structures to provide roosting opportunities. This will be controlled by condition. Proposed floodlights have the potential to increase artificial light levels on bat foraging and commuting habitat to the west of the site – identified in the Bat Activity Report as areas of moderate levels of bat activity. To avoid negative impacts, a condition is recommended requiring the production of a Lighting Design Strategy for Bats.
- 10.3.9. A further condition is recommended to control a type of invasive non-native plant species known as Cotoneaster.

#### **10.4. Sustainable Transport and Travel Planning**

- 10.4.1. Policy T2 of the Core Strategy requires new development to be sufficiently accessible by sustainable means of transport, provides safe and secure access for pedestrians, cyclists and people with impaired mobility and sufficient parking for cars and bikes. Detailed guidance and standards are provided in the Transport SPD 2023.
- 10.4.2. The location of the proposed development at the Roundhouse, Wellington Road is highly accessible in Leeds City Centre. Currently, the site is only accessible from Wellington Road, but the completion of the Doncaster Monkbridge residential development immediately to the south will see a footpath connection from Wellington Road through to Wellington Place. The footpath alongside the Leeds-Liverpool Canal will also provide an attractive route into the heart of the City Centre, and out towards the Kirkstall Valley for pedestrians and cyclists. The site is also served by bus stops on Wellington Road offering 11 different bus routes, and the train station is only 15 minutes walk away.
- 10.4.3. The applicant has committed to provide the transport improvements to the scheme sought by the Highways Service. These have been set out on the revised masterplan drawing, but further technical highways drawings are awaited. The following matters are agreed:
  - i. Clear pathways are set out on site for pedestrians to walk safely between the building and the courts and car parking spaces

- ii. Cycle parking for 40 short stay visitor cycles will be provided. With the total area of the 10 courts measuring approximately 1,930sqm, this would exceed the 1 space per 50sqm expected by the Transport SPD for indoor leisure development. Also, if the 10 courts were fully booked, all for doubles, and in the unlikely worse case scenario that they all arrived by bike, it would generate a need for 40 bike spaces. As such, it is considered that 40 short stay visitor cycle spaces is sufficient.
- iii. Cycle parking for 10 long stay cycles will be provided within the roundhouse building, offering cover and security. The Transport SPD expects a ratio of 1 space per 5 employees. It is estimated that there will be approximately 10 – 20 employees, so the cycle provision comfortably exceeds the expectation of the Transport SPD.
- iv. Motorcycle parking for 14 motorcycles will be provided. This exceeds the requirement of the Transport SPD, which would only require 1 or 2 spaces (ratio of 1 space / 2000sqm).
- v. Car parking will be provided for 20 - 30 cars, accessed off a one-way internal circulation road of c.3.3m width, This includes 4 disabled spaces close to the main entrance. The exact number of car parking spaces will be finalised prior to determination
- vi. Locations for a servicing bay and an enclosure for bin storage to the rear of the roundhouse building have been agreed. Exact details will be finalised prior to determination.
- vii. The applicant has agreed to provide off-site highway works including laying out of a 3m footway/cycleway around the Wellington Road and Graingers Way frontages and the transfer of land to assist with the replacement of the footbridge over the A58 (Wellington Road).

10.4.4. Altogether it is considered that the scheme will be safe, effective and sustainable in transport terms in accordance with planning policy.

#### Travel Planning

10.4.5. The developer submitted a travel plan and agreed to pay the Travel Plan Review fee of £3,682 (subject to indexation). The Influencing Travel Behaviour Team has asked for amendments to the Travel Plan including guaranteeing the budget for the Travel Plan Coordinator (TPC), confirming the person responsible for appointing the TPC and updating target timeframes. These changes are underway and will need to be agreed prior to determination.

### **10.5. Planning Obligations**

10.5.1. The Council's adopted policies would result in the following Section 106 matters, which are considered to be necessary, directly related to the development and fairly and reasonably related in scale and kind to the development in accordance with the tests set out at regulation 122 of the Community Infrastructure Levy Regulations 2010:

- i. Travel Plan Review fee of £3,682
- ii. Transfer of land for A58 footbridge improvement
- iii. Monitoring fee

## **11. Conclusion**

- 11.1. On the principal of development it is considered that the revised scheme is acceptable bringing a sport/leisure use that would contribute positively to health, fitness and wellbeing of Leeds in accordance with policies SP3, CC1 and P9 of the Core Strategy. The loss of potential for housing development would only cover half of the housing allocation (HG2-113) with the remaining half retained for housing development, and the proposed use is for a temporary 10 year period meaning that there will be potential for housing development on all of the allocation in the long term. As such, the principle of development is considered to accord with Policy GP1 governing the release of allocations for other uses.

### **Background Papers**

Application files 23/01249/FU and 23/01250/LI

### **Appendices**

Appendix 1: Draft Conditions for application 23/01249/FU

Appendix 2: Draft Conditions for application 23/01250/LI





Appendix A (draft conditions for application 23/01249/FU)

- 1) The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Imposed pursuant to the provisions of Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

- 2) The development hereby permitted shall be carried out in accordance with the approved plans listed in the Plans and Specifications above.

For the avoidance of doubt and in the interests of proper planning.

- 3) Except with the written approval of the local planning authority, padel courts shall not be used outside the hours of:  
08.00 to 2100 hours Mondays to Saturdays  
09.00 to 18.00 hours, Sundays or Public Holidays

In the interests of amenity

- 4) Prior to first use, a noise management plan shall be submitted and approved in writing that sets out how noise impact from the use of the Padel courts shall be managed. The noise management plan shall include details of how the site will be supervised during use, the hours of use and a complaints procedure.

In the interests of amenity.

- 5) Plant and machinery operated from the site shall limit noise to a level no higher than the existing background noise level (L90) when measured at noise sensitive premises, with the measurements and assessment made in accordance with BS4142:2014. The rating level shall include the addition of any character corrections as appropriate. If the character is unknown at the design stage or cannot be evidenced then a penalty of 5dB should be applied to take into account of potential corrections.

In the interests of residential amenity.

- 6) Any music or amplified sound including tannoy systems used shall not be audible from any point on the site boundary.

In the interests of residential amenity.

- 7) No development to take place within the external area to the east of the Roundhouse until the developer has secured the implementation of a programme

of archaeological recording. This recording must be carried out by an appropriately qualified and experienced archaeological organisation or consultant, in accordance with a written scheme of investigation which has been submitted by the applicant and approved in writing by the Local Planning Authority.

In the interests of archaeology and local history

- 8) No building works shall take place until details of the design, location and timescales for providing access for bats to potential roost sites in buildings and bat boxes on trees have been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details.

Such provision is an integral part of the building fabric and in the interest of nature conservation it is important to provide potential roosting sites for bats which are known to feed over the site. In the interests of biodiversity.

- 9) No demolition or development shall take place until a lighting design strategy for bats has been submitted to and approved in writing by the Local Planning Authority. The strategy shall include full details of measures to ensure that there is no adverse impact on bats, monitoring to assess effectiveness of these measures and a timetable for implementation. The works shall be implemented in accordance with the approved details.

Failure to ensure adequate protection and enhancement measures for bats during construction works could harm protected species.

- 10) Prior to the commencement of development a Method Statement for the control and eradication of cotoneaster shall be submitted to and approved in writing by the local planning authority. The Method Statement will include post-treatment monitoring of the site to ensure a continuous 12-month period of time occurs where none of the Target Species is identified growing on the whole site, if any Target Species is identified as growing on-site during the 12-month monitoring period then treatment shall resume and continue until a continuous 12-month period with no Target Species occurs. The agreed Method Statement shall thereafter be implemented in full.

Reason: to control the spread of non-native invasive plant species in accordance with the Wildlife & Countryside Act 1981 (as amended) and BS 42020:2013.

- 11) Any gates at the site entrance should remain open throughout the operation hours to prevent vehicles waiting to enter the site blocking other traffic on the road.

In the interests of highway safety.

- 12) Development shall not be occupied until details (including location and size) of proposed In and Out signage, clearly visible to motorists, have been submitted to and approved in writing by the Local Planning Authority. The approved works shall be completed before the development is occupied and retained for the lifetime of the development.

To ensure the free and safe use of the highway.

- 13) Development shall not be occupied until a Car Park and Servicing Management Plan (including timescales) has been submitted to and approved in writing by the Local Planning Authority. The plan shall include the following information: (INSERT SPECIFIC REQUIREMENTS). The plan shall be fully implemented, and the development thereafter operated in accordance with the approved timescales.

To ensure the free and safe use of the highway.

- 14) Development shall not commence until a statement of construction practice has been submitted to and approved in writing by the Local Planning Authority, the statement of construction practice shall include full details of:

a) the construction vehicle routing, means of access, location of site compound, storage and parking (including workforce parking), means of loading and unloading of all contractors' plant, equipment, materials and vehicles and associated traffic management measures.

b) methods to prevent mud, grit and dirt being carried on to the public highway from the development hereby approved.

c) measures to control the emissions of dust and dirt during construction.

d) how the statement of construction practice will be made publicly available by the developer.

The approved details shall be implemented at the commencement of works on site and shall thereafter be retained and employed until completion of the works on site. The Statement on Construction practice shall be made publicly available for the lifetime of the construction phase of the development in accordance with the approved method of publicity.

The carrying out of the development could result in significant harm to the amenities of local residents and highway safety, and accordingly details of construction practice is required to be agreed prior to commencement of works in order to protect such interests.

- 15) Prior to occupation of the development, the off-site highway works as shown in principle on plan comprising \*\*\*\* at \*\*\*\* shall be fully delivered.

To ensure the free and safe use of the highway

Informative: In relation to Condition (\*\*\*\*) of this permission, the applicant must enter an agreement with the Council under Section 278 of the Highways Act 1980. The applicant is advised to make early contact with the Highway and Transportation Service by emailing the details to [S278Agreements@Leeds.gov.uk](mailto:S278Agreements@Leeds.gov.uk). Further information is available on the council's website at <https://www.leeds.gov.uk/parking-roads-and-travel/licences-and-permits>.

- 16) The development hereby approved shall not be occupied until full details and a scheme for provision of Electric Vehicle Charging Points, cable enabled spaces and associated infrastructure have been submitted to and approved in writing by the Local Planning Authority. The facilities shall be provided prior to first occupation of the development, retained and maintained thereafter as approved for the lifetime of the development.

In the interest of promoting low carbon transport.

- 17) The development hereby permitted shall not be occupied until details for the provision of bin stores (including siting, materials and means of enclosure) and (where applicable) storage of wastes and access for their collection shall be submitted to and approved in writing by the Local Planning Authority. The approved measures shall be implemented in full before the use commences and shall be retained thereafter for the lifetime of the development.

To ensure adequate measures for the storage and collection of wastes are put in place. In the absence of appropriate measures residential amenity could be adversely affected.

Informative: In order to discharge condition () of this permission, it is necessary to obtain separate approval from the Waste Management department in relation to refuse storage details. The applicant is advised to make early contact with [Bin.Deliveries@leeds.gov.uk](mailto:Bin.Deliveries@leeds.gov.uk) prior to submission of condition discharge application.

- 18) The development hereby permitted shall not be occupied until full details of both hard and soft landscape works, including a dated implementation programme (inclusive of any phasing), have been submitted to and approved in writing by the Local Planning Authority. Hard landscape works shall include
- (a) proposed finished levels and/or contours,
  - (b) boundary details, means of enclosure and retaining structures,
  - (c) car parking layouts,
  - (d) other vehicle and pedestrian access and circulation areas,
  - (e) hard surfacing areas,

(f) minor artefacts and structures (e.g. furniture, play equipment, refuse or other storage units, signs, lighting etc.),

(g) existing trees with Root Protection Areas (RPAs) and all other retained vegetation.

Soft landscape works shall include

(h) planting plans (display existing trees with Root Protection Areas (RPAs) and all other retained vegetation),

(i) written specifications (including soil depths, cultivation and other operations associated with plant and grass establishment) and

(j) schedules of plants noting species, planting sizes, root packaging and proposed numbers/densities,

(k) drainage proposals.

All hard and soft landscaping works shall be carried out in accordance with the approved details, approved implementation programme, British Standard BS 4428:1989 (Code of Practice for General Landscape Operations) and in accordance with Leeds City Council website landscape guidance under "Landscape Planning and Development". The landscaping works shall be maintained in accordance with the approved details for the lifetime of the development.

To ensure the provision and establishment of acceptable landscaping.

- 19) A landscape management plan, including long term design objectives, management responsibilities and maintenance schedules shall be submitted to and approved in writing by the Local Planning Authority prior to the occupation of the development. The Management Plan must conform to Leeds City Council's Landscape Management Plans (Landscape Guidance No.2) and associated checklist. The landscape management plan shall be for the lifetime of the development and shall be carried out as approved.

To ensure successful aftercare of landscaping.

- 20) The changing rooms, toilets and communal areas shall be designed to be accessible to all, including wheelchair users.

In the interests of equality of access.

- 21) Development (excluding demolition) shall not commence until a Phase I Desk Study has been submitted to, and approved in writing by, the Local Planning Authority and:
- (a) Where the approved Phase I Desk Study indicates that intrusive investigation is necessary, development (excluding demolition) shall not commence until a Phase II Site Investigation Report has been submitted to and approved in writing by the Local Planning Authority;

(b) Where remediation measures are shown to be necessary in the Phase I/Phase II Reports and/or where soil or soil forming material is being imported to site, development (excluding demolition) shall not commence until a Remediation Strategy demonstrating how the site will be made suitable for the intended use has been submitted to and approved in writing by the Local Planning Authority. The Remediation Strategy shall include a programme for all works and for the provision of Verification Reports.

It is strongly recommended that all reports are prepared and approved by a suitably qualified and competent person.

To ensure that the presence of contamination is identified, risks assessed and proposed remediation works are agreed in order to make the site 'suitable for use' with respect to land contamination.

- 22) If remediation is unable to proceed in accordance with the approved Remediation Strategy, or where significant unexpected contamination is encountered, or where soil or soil forming material is being imported to site, the Local Planning Authority shall be notified in writing immediately and operations on the affected part of the site shall cease. The affected part of the site shall be agreed with the Local Planning Authority in writing. An amended or new Remediation Strategy and/or Soil Importation Strategy shall be submitted to and approved in writing by the Local Planning Authority prior to any further remediation works which shall thereafter be carried out in accordance with the revised approved Strategy. Prior to the site being brought into use, where significant unexpected contamination is not encountered, the Local Planning Authority shall be notified in writing of such.

It is strongly recommended that all reports are prepared and approved by a suitably qualified and competent person.

To ensure that any necessary remediation works are identified to make the site 'suitable for use' with respect to land contamination.

- 23) Remediation works shall be carried out in accordance with the approved Remediation Strategy. On completion of those works, the Verification Report(s) shall be submitted to the Local Planning Authority in accordance with the approved programme. The site or phase of a site shall not be brought into use until such time as all verification information has been approved in writing by the Local Planning Authority.

It is strongly recommended that all reports are prepared and approved by a suitably qualified and competent person.

To ensure that the remediation works are fully implemented as agreed and the site has been demonstrated to be 'suitable for use' with respect to land contamination.

- 24) The site shall be developed with separate systems of drainage for foul and surface water on and off site. The separate systems should extend to the points of discharge to be agreed.

In the interest of satisfactory and sustainable drainage.

- 25) There shall be no piped discharge of surface water from the development prior to the completion of surface water drainage works, details of which will have been submitted to and approved by the Local Planning Authority. If discharge to public sewer is proposed, the information shall include, but not be exclusive to:- a) evidence to demonstrate that surface water disposal via infiltration or watercourse are not reasonably practical; b) evidence of existing positive drainage to public sewer and the current points of connection; and c) the means of restricting the discharge to public sewer to the existing rate less a minimum 30% reduction, based on the existing peak discharge rate during a 1 in 1 year storm event, to allow for climate change.

To ensure that no surface water discharges take place until proper provision has been made for its disposal and in the interest of sustainable drainage

**Reason(s) for granting consent:-**

**For information:-**

- 1) The Council engages with all applicants in a positive and proactive way through specific pre-application enquiries and the detailed advice available on the Council's website and further discussion where appropriate to produce an acceptable development. For this particular application, positive discussions took place which resulted in further information being submitted to allow the application to be approved.
- 2) The contractor must ensure compliance with current legislation on noise and dust control including the Environmental Protection Act 1990 and the Control of Pollution Act 1974. Relevant Codes of Practice set out procedures for dealing with the control of noise on construction and demolition sites are contained in BS5228: 2009 Noise and Vibration Control on Construction and Open Sites.

- 3) The applicant/developer is advised to contact the Canal & River Trust's Works Engineering Team via switchboard on 03030404040 or via email [tpwnorth@canalrivertrust.org.uk](mailto:tpwnorth@canalrivertrust.org.uk) in order to ensure that any necessary consents for works to the north east of the site are obtained and that the works would comply with the Trust's "Code of Practice for Works affecting the Canal & River Trust.
- 4) In relation to Condition () of this permission, the applicant must enter an agreement with the Council under Section 278 of the Highways Act 1980. The applicant is advised to make early contact with the Highways and Transportation Service by emailing the details to [S278Agreements@Leeds.gov.uk](mailto:S278Agreements@Leeds.gov.uk) as negotiating and entering a Section 278 Agreement generally takes 12 months; but, this can be longer depending on the complexity of the scheme. Further information is available on the Council's website at <https://www.leeds.gov.uk/parking-roads-and-travel/licences-and-permits>.
- 5) In order to discharge condition () of this permission, it is necessary to obtain separate approval from the Waste Management department in relation to refuse storage details. The applicant is advised to make early contact with [Bin.Deliveries@leeds.gov.uk](mailto:Bin.Deliveries@leeds.gov.uk) prior to submission of condition discharge application.
- 6) All reports addressing land contamination should be compiled in accordance with best practice and with the National Planning Policy Framework and policies Land 1 of the Natural Resources and Waste Local Plan 2013 and GP5 of the Unitary Development Plan Review 2006.

Prior to preparing any reports in compliance with conditions related to land contamination the applicant is also advised to refer to the latest version of the Yorkshire and Lincolnshire Pollution Advisory Group technical guidance for developers, landowners and consultants, as noted below:

- Development on Land Affected by Contamination
- Verification Requirements for Cover Systems
- Verification Requirements for Gas Protection Systems

Where C\_SOIL has been placed on the planning permission, guidance on the required information to submit is available in the Verification Requirements for Cover Systems guidance.

The latest version of this guidance and additional information is available at [www.leeds.gov.uk/contaminatedland](http://www.leeds.gov.uk/contaminatedland)



- 7) The applicant is advised that remediation of any contaminated site is required to a standard such that the site is 'suitable for its proposed use' in accordance with the National Planning Policy Framework (NPPF) and policies Land 1 of the Natural Resources and Waste Local Plan 2013 and GP5 of the Unitary Development Plan Review 2006.

Remediation may include the requirement for the importation of suitable soils and/or soil forming materials, an appropriately designed capping layer and satisfactory gas protection measures.

In order that the council can confirm that the site has been demonstrated as suitable for use, verification information in line with the approved Remediation Strategy must be submitted to the Local Planning Authority for approval in writing. Without submission of evidence to support the discharge of conditions relating to verification eg C\_VERI, C\_SOIL, C\_LUNX, there may arise delays to condition discharge, failure of property sale, liability issues and enforcement action including action under Part 2A of the Environmental Protection Act 1990.

As noted in the NPPF, where a site is affected by contamination issues, responsibility for securing a safe development rests with the developer and/or landowner.

- 8) An appropriate survey should be undertaken to identify any asbestos containing materials in existing structures/buildings, including fly-tipped waste or rubble from the demolition process. Asbestos containing materials must be safely removed prior to demolition and site clearance, or conversion of existing structures/buildings, to avoid causing risks to public health and the environment. Asbestos-contaminated waste must be disposed of appropriately at a licensed waste facility. The legal requirements for managing and working with asbestos are set out in the Control of Asbestos Regulations 2012.

Appendix B (Draft Conditions for application 23/01250/LI)

- 1) The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Imposed pursuant to the provisions of Section 18 of the Planning (Listed Building and Conservation Areas) Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

- 2) The development hereby permitted shall be carried out in accordance with the approved plans listed in the Plans and Specifications above.

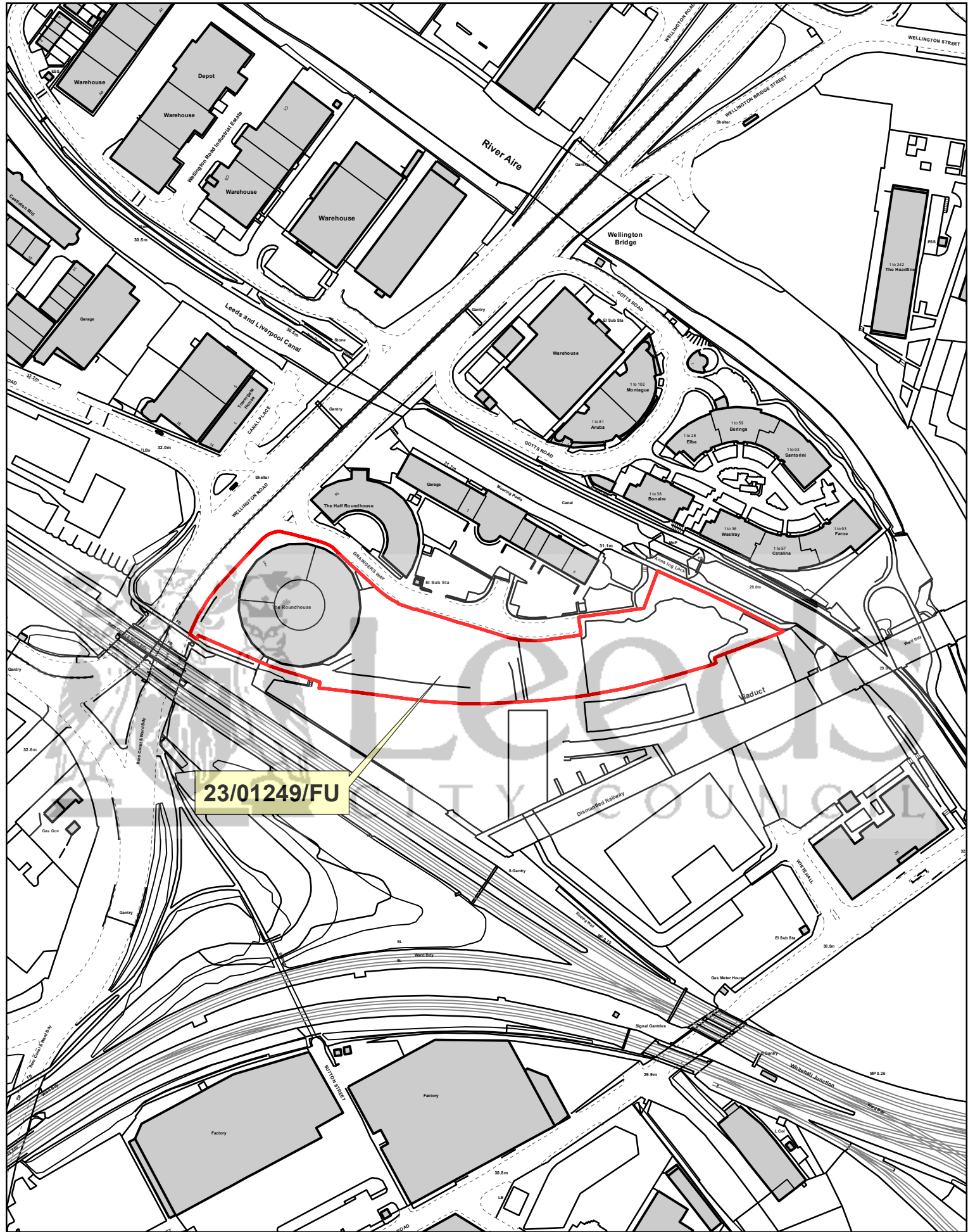
For the avoidance of doubt and in the interests of proper planning.

- 3) Works to remove the existing roller shutters and reinstate the brickwork and windows to match the original shall be implemented within 5 years of the commencement of works.

To enhance the historic character of the building

**For information:-**

- 1) The Council engages with all applicants in a positive and proactive way through specific pre-application enquiries and the detailed advice available on the Council's website and further discussion where appropriate to produce an acceptable development. For this particular application, positive discussions took place which resulted in further information being submitted to allow the application to be approved.



23/01249/FU

# CITY PLANS PANEL



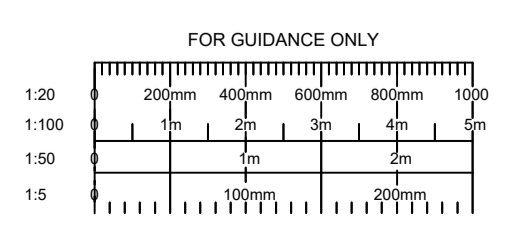
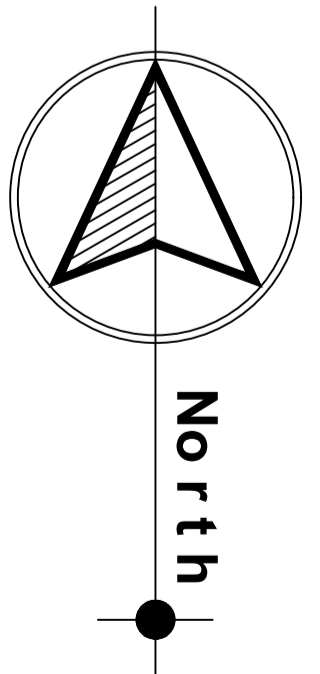
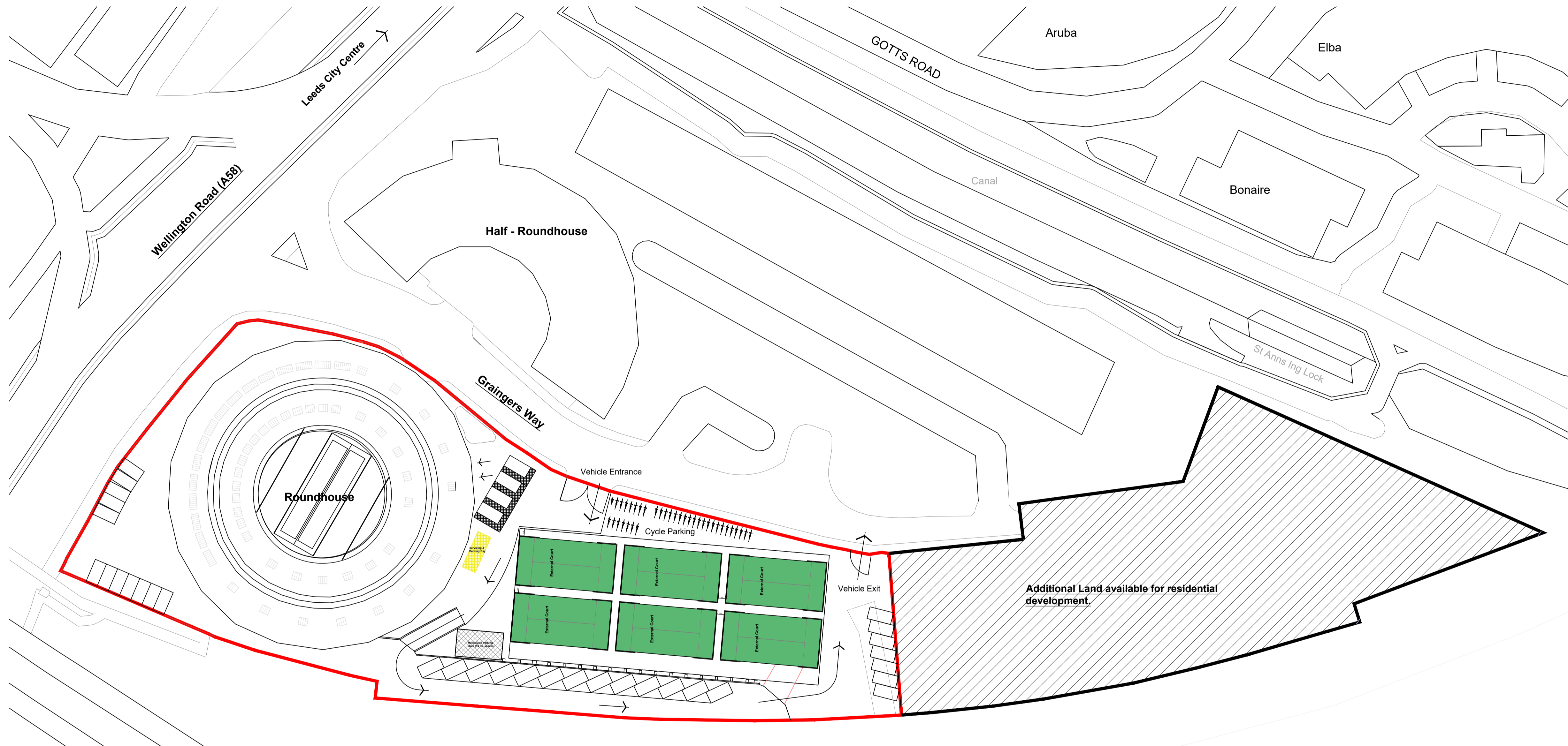
The following risks are identified as unusual or unfamiliar to a competent contractor

**CONSTRUCTION RISKS**  
There are no significant or unfamiliar risks

**DEMOLITION RISKS (FUTURE)**  
There are no significant or unfamiliar risks

It is assumed that all work will be carried out by a competent contractor working, where appropriate, to an approved method statement

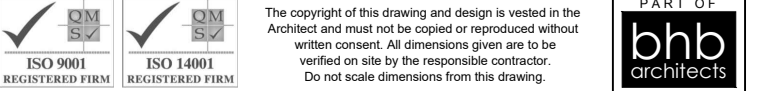
Notes:  
Site Area: 8,737sq.m  
(Overall area above includes the Roundhouse)  
Residential Land: 4590 sq.m  
Phase 2 has been excluded to allow for residential development.



A	Phase 2 has been excluded to allow for residential development.	18/07/23	OT
REV	DETAILS	DATE	CHECKED



Leeds Roundhouse, Graingers Way, Leeds		DRAWING NO	
Proposed Masterplan		3950-802	
We Are Padel		REV	DATE
Planning		A	Aug 23
DRAWN	CHECKED	SCALE	
OT	SB	1:500 @ A1	





Originator: Andrew Perkins

## Report of the Chief Planning Officer

### CITY PLANS PANEL

Date: 15<sup>th</sup> February 2024

**Subject: Planning Application 23/06266/FU for the construction of an office building with associated facilities, parking and landscaping at a site to the north of Whitehall Road (land at the former Doncaster Monk Bridge Works) Leeds.**

### APPLICANT

BAM Monk Bridge Limited

### DATE VALID

16 October 2023

### TARGET DATE

15 January 2023

Electoral Wards Affected:

Hunslet & Riverside

### Specific Implications For:

Equality and Diversity

Community Cohesion

Narrowing the Gap

**RECOMMENDATION: Defer and Delegate to the Chief Planning Officer for approval, subject to the conditions at Appendix 1 (and any amendment to or addition of others which the Chief Planning Officer considers appropriate), and subject to resolving technical details regarding highways matters and Active Travel England comments and also subject to the completion of a Section 106 agreement to secure the following:**

- 1.0.91 Biodiversity Units to be Habitat Units delivered off site in the same locality – £25,000**
- 2. On site Public Realm areas are publicly accessible**
- 3. Travel Plan review fee – £5504**
- 4. Contribution for free trial membership and usage of the car club by occupiers of the development and/or other sustainable travel measures for the employees e.g. public transport tickets, hire bikes – £22,000**
- 5. Globe Road/Whitehall Road Junction improvements – £188,250**
- 6. Management Plan to cover maintenance of Whitehall**
- 7. Wayfinding – £16,000**
- 8. Employment and training of local people**

## **9. Monitoring fees.**

**In the circumstances where the Section 106 has not been completed within 3 months of the Panel resolution to grant planning permission, the final determination of the applications shall be delegated to the Chief Planning Officer**

### Conditions

A list of draft Conditions for the application is provided in the Appendix 1 of this report.

## **1.0 INTRODUCTION:**

- 1.1 The proposal is put before City Plans Panel, in accordance with the Officer Delegation Scheme 1(g) as the Chair in conjunction with the Chief Planning Officer considers that the application should be referred to the relevant Plans Panel for determination because of the significance, impact or sensitivity of the proposal.
- 1.2 This site has a previous planning approval as part of a wider masterplan for the Former Doncaster Monkbridge Works (06/02880/OT) for a 10-storey office building on the site. This permission was granted with reserved matters in October 2015 (13/02017/RM), with pre commencement conditions being discharged in July 2017 (17/03305/COND). Since securing planning permission, this site was marketed on a pre-let basis, however at that time the site did not appear attractive to the markets and the site has laid dormant despite the developments which have been carried out around it.
- 1.3 The application site is now considered much more connected to Leeds City Centre and there is a significant opportunity to reposition this remaining element of the Former Doncaster Monkbridge Works site (Latitude Yellow) in the marketplace to deliver a sustainable Grade A office building.
- 1.4 The Applicant's team presented the emerging pre-application proposals to City Plans Panel on 10<sup>th</sup> August 2023. At the meeting Members were generally supportive of the proposal. Member's comments from the City Plans Panel minutes are set out below in paragraph 5.2.

## **2.0 PROPOSAL:**

- 2.1 The proposals seek to create a 12-storey office building with a public ground floor café/co working space and an external employee roof terrace which will be landscaped.
- 2.2 A total of 10 car parking spaces (including 2 disabled spaces) are proposed within the ground floor undercroft parking area, of which Electric Vehicle Charge points will be provided to all spaces. A total of 138 long stay cycle spaces are to be provided across ground and mezzanine levels. In addition, 20 short stay cycle spaces will be provided through 10 Sheffield stands within the landscaped areas.
- 2.3 The development would also feature new landscaping, public realm and improvements to cycle and pedestrian infrastructure along Whitehall Road. The landscaping would also incorporate new tree planting to the east and south with a new public space to the west of the building, featuring a mixture of soft and hard landscaping.

2.4 The application is supported with the following documents:

- I. Scaled Drawings
- II. Design And Access Statement
- III. Planning Policy Statement
- IV. Air Quality Assessment
- V. Ecological Impact Assessment
- VI. Biodiversity Metric 4.0 Calculation Tool
- VII. Desktop Solar Glare Report
- VIII. Flood Risk Assessment
- IX. Drainage Strategy
- X. Heritage Statement
- XI. Groundwater Risk Assessment
- XII. Lighting Assessment
- XIII. Noise Impact Assessment
- XIV. Phase 1 & 2 Ground Report
- XV. Bay Materiality Studies
- XVI. Travel Plan
- XVII. Pedestrian Level Wind Microclimate Assessment
- XVIII. Sustainability Appraisal
- XIX. BREEAM Pre-Assessment
- XX. Statement Of Community Involvement

### **3.0 SITE AND SURROUNDINGS:**

- 3.1 Historically the site was home to the Monk Bridge Iron Company and then the Monk Bridge Iron Company, which created various high quality Iron and alloy steels. The site is cleared and located to the west of the City and to the north of Whitehall Road and forms part of the wider Latitude development, which is located to the east of the elevated Leeds-Bradford railway line and west of the River Aire and Leeds-Liverpool Canal. The Latitude development consists of a range of heights from 8 storeys of Latitude Red (east of this site) to 31 storeys of the Latitude purple A (north east of this site).
- 3.2 The site totals (approx.) 0.31 hectares and falls inside of the defined city centre of Leeds. The site is enclosed by post rail fencing and currently consists of predominantly a grassed area. The site is bounded by Whitehall to the north and east which also serves the other Latitude development plots and Whitehall Road to the south and the railway embankment to the west.
- 3.3 The area around the site is evolving and a number of consented/ in construction developments are noted which are Get Living to the south of Whitehall Road which ranges from 8 to 23 storeys, Monkbridge development which ranges from 12 to 22 storeys and the development of Latitude which range from 8 to 31 storeys. The general theme of architecture along Whitehall Road is modern and contemporary, aside from the Monk Bridge Viaduct and Monk Bridge which are both grade II listed.
- 3.4 Pedestrian links are located to the south along Whitehall Road and also along Monk Bridge Viaduct leading in and out of the city centre.
- 3.5 The site forms part of a wider site identified in the Site Allocations Plan (SAP) as a Mixed Use Allocation site ref. MX1-11 for at least 50380sqm office use and 463 Residential units. This allocation involves the other areas to the north and east which have already been developed or are in the process of being developed, which equates

to a site allocation of 3.49 hectares. The entire site is located within Flood Risk Zone 1.

#### **4.0 RELEVANT PLANNING HISTORY:**

- 4.1 17/03305/COND - Consent, agreement or approval required by conditions 8, 13, 14, 15, 16, 17, 18, 19, 26, 27, 30, 49 and 50 of Planning Application 16/04118/OT – Split decision – 17.07.2017
- 4.2 13/02017/RM - Reserved matters application for 10 storey office block with basement car park and roof top plant room – Approved – 12.10.2015
- 4.3 16/04118/OT - Amendments to conditions 4 (phasing plan), 46 (Design Statement/Design Code) and 53 (plans schedule) of approval 06/02880/OT – Approved – 21.10.2016
- 4.4 06/02880/OT - Outline application to layout access and erect multi level mixed use development for residential and office uses up to 33 storeys high, with ancillary class A1, A2, A3, A4, A5, D1 and D2 uses and associated car parking and landscaped areas. – Approved – 07.09.2007

#### **5.0 HISTORY OF NEGOTIATIONS:**

- 5.1 The proposals have been the subject of pre-application discussions between the Developer, their Architects, and Local Authority Officers since September 2022. The discussions as part of this preapplication focused on the design, massing and layout of the scheme including relationship with Whitehall Road. Extensive wind testing and reviewing of mitigation measures. Highway matters including access and servicing strategy and traffic modelling. Landscaping matters including landscaping strategy, tree planting and amenity spaces.
- 5.2 The preapplication proposals were presented to City Plans Panel on 10<sup>th</sup> August 2023. Members made the following comments, which are relevant to this aspect of the development; Members were generally supportive of the proposal and welcomed the introduction of the roof terrace and sustainability issues. Members also hoped the proposal would reflect the heritage of the site with recognition to the history of the site.

#### **6.0 PUBLIC/LOCAL RESPONSE:**

- 6.1 Planning application publicity consisted of:  
Site Notices posted adjacent and around the site on 07.11.2023  
Press Notice published 17.11.2023
- 6.2 As part of the application publicity no letters of comment have been received and the publicity period closed on 20.01.2023.
- 6.3 Hunslet & Riverside Ward Councillors have been alerted to this application via email, no responses were received

#### **7.0 CONSULTATION RESPONSES:**

- 7.1 **Network Rail** – No objection, conditions recommended.  
Response: The required planning conditions will be applied



- 7.2 **West Yorkshire Police** – No objection in principle to the proposal providing that the recommendations are considered and secured via condition as part of any planning approval.  
Response: The required planning condition will be applied
- 7.3 **Active Travel England** - ATE recommend any decision on this application is deferred until more information is provided to help address the following issues:
- There is a need to further develop the analysis within the Transport Assessment and crucially the Travel Plan to better reflect active travel journey and the requirement to increase these modes in goals, aims and targets.
  - Better provision for cycle parking on the ground floor and an understand of the security of the short stay spaces is required.
- Response: These matters are being addressed in consultation with Active Travel England but will not affect the overall design and nature of the proposals presented to Panel
- 7.4 **LCC Highways** – see paragraphs 10.34 to 10.41 below
- 7.5 **LCC Influencing Travel Behaviour** - that there is a requirement for the following obligations: A Travel Plan Review fee of £5504 (subject to an annually increase to reflect inflation). A Contribution of £22,000 free trial membership and usage of the car club by office occupiers of the development. The S106 should make clear that if the contribution is not spent on car club trial for the office occupiers, the site wide travel plan co-ordinator can use the contribution for other sustainable travel measures for the employees e.g. public transport tickets, hire bikes, etc.  
Responses: These obligations will be secured via the legal agreement.
- 7.6 **LCC Sustainable Development Unit (Climate Change Officer)** - states that the submitted Energy Strategy states its intent to comply with the Leeds Core Strategy Policies EN1 (i), (ii), EN2 and EN4.  
Response; to allow these matters to be addressed fully at the appropriate design stage/s these matters will be controlled via Planning Conditions.
- 7.7 **LCC District Heating Network** - This site is not within connection distance of a live heat network. However, the council is working on a large-scale extension to its Leeds PIPES district heating network, the route for which would be close to this site. It is anticipated that on the basis of the council securing sufficient funding, the network will be extended in the next three years, with a rough heat-on date of 2025. It is recommended therefore that this scheme be designed for a connection, ideally at the time of completion or at the very least in the future. In addition, the government's heat network zoning scheme is due to come into effect in 2025, which is likely to mandate connection of schemes such as this to heat networks.  
Response: The site features a connection point for future connection to the heat network, which will be secured via condition.
- 7.8 **LCC Environmental Health Services (Pollution Control)** - Having reviewed the proposals and the noise assessment submitted in support of this application, we have no adverse comment to make. Recommendations made in the noise assessment with regard to fixed plant emissions and building design demonstrate that no adverse impacts are likely to arise.
- 7.9 **LCC Sustainability - Design Team** – proposal follows pre application advice. All external materials should be conditioned, and conditions are put into any approval for 1:20 details to secure the reveal depths and the like. We suggest asking for the details through all the major section points through the building. These would include all

openings, parapets, doors and the like. We would suggest a minimum reveal depth of 215mm.

Response: Such conditions will be applied.

- 7.10 **LCC Sustainability – Conservation Team** - state that the proposal does not have any impact on nearby heritage assets and would have a neutral impact upon the character and significance of the Holbeck and Canal Wharf Conservation Areas.
- 7.11 **LCC Sustainability - Nature Team** - state they have no objections to the proposal subject to signing a legal agreement for Biodiversity improvements works off site (£25,000). The shortfall of 0.91 Habitat Units could be delivered on Leeds City Council's or a partner's land in the same locality and secured through the S106 agreement. Conditions are required regarding a Protected Species: Reasonable Avoidance Measures Method Statement, Lighting Design Strategy for Bats and details of integral bat roosting and integral bird nesting features  
Response: Such relevant conditions will be applied and the Biodiversity works will be secured via a legal agreement.
- 7.12 **LCC Sustainability - Landscape Team** - state that the details of the landscape scheme need to be addressed via Conditions  
Responses: Such relevant conditions will be applied
- 7.13 **LCC Access Officer** - a condition for landscaping should be included which states that it needs to meet BS8300 and a condition is also required for all public seating.  
Response: Such condition will be applied.
- 7.14 **LCC Flood Risk Management** - require conditions to cover details of surface water disposal.  
Response: Such condition will be applied.
- 7.15 **LCC Environmental Studies – Transport Strategy** - We have no objection to this proposed development, but Environmental Health may wish to comment on the potential issue of commercial noise etc.
- 7.16 **LCC Contaminated Land Team** - state that conditions to cover the submission of a Remediation Strategy is required.  
Response: such condition will be applied.
- 7.17 **Yorkshire Water Service Ltd** - require conditions to cover details of surface water disposal.  
Response: Such condition will be applied.
- 7.18 **West Yorkshire Archaeological Service** - There are no archaeological implications to the proposed development.
- 7.19 **LCC Local Plans** – No response received
- 7.20 **LCC Cycling Officer** – No response received
- 7.21 **Neighbourhood Forum** - No response received
- 7.22 **Canal & River Trust** – no comments to make on the proposal
- 7.23 **Employment and Skills** - No response received

- 7.24 **Ramblers Association** - No response received
- 7.25 **Health Partnerships** - No response received
- 7.26 **Public Health** - No response received
- 7.27 **Environment Agency** - No response received
- 7.28 **West Yorkshire Ecology** - No response received
- 7.29 **Yorkshire Wildlife Trust** - No response received
- 7.30 **Historic England** – No comments to make
- 7.31 **Natural England Consultation Service** - No response received

## **8.0 RELEVANT PLANNING POLICIES:**

### **8.1 Statutory Context**

- 8.1.1 Section 38(6) of the Planning and Compulsory Purchase Act states that for the purpose of any determination to be made under the Planning Acts, the determination must be made in accordance with the plan, unless material considerations indicate otherwise. The development plan currently comprises the adopted Local Development Framework Core Strategy as amended (2019), those policies saved from the Leeds Unitary Development Plan (Review 2006) (UDP), the Aire Valley Leeds Area Action Plan (2017), the Natural Resources and Waste Local Plan, the Site Allocations Plan (July 2019) and any made Neighbourhood plan.

### **8.2 Development Plan**

- 8.2.1 **Leeds Core Strategy (as amended 2019)** sets out strategic level policies and vision to guide the delivery of development investment decisions and the overall future of the district. Relevant Core Strategy policies will include those outlined below.

Spatial Policy 1 Location and scale of development.

Spatial Policy 2 hierarchy of centres and spatial approach to retailing, offices, intensive leisure and culture

Spatial Policy 3 City Centre Development

Spatial Policy 8 Economic development priorities

Spatial Policy 9 Employment

Spatial Policy 11 Transport infrastructure investment priorities such as pedestrian improvements

Policy CC1 City Centre Development

Policy CC2 City Centre South

Policy CC3 Improving connectivity between the City Centre and Neighbouring Communities.

Policy EC2 Office Development

Policy P8 Sequential and Impact Assessments for Main Town Centre Uses

Policy P10 Design

Policy P11 Heritage

Policy P12 Landscape

Policy T1 Transport Management

Policy T2 Accessibility Requirements and New Development

Policy EN1 Carbon Dioxide Reduction

Policy EN2 Sustainable Design and Construction  
Policy EN4 District Heating  
Policy EN5 Managing Flood Risk  
Policy G8 Protection of Important Species and Habitats  
Policy G9 Biodiversity Improvements  
Policy ID2 Planning Obligations and Developer Contributions

8.3 Relevant Saved Policies from the **Leeds Unitary Development Plan (UDP)** are:

Policy GP1 - Land use and the Proposals Map  
Policy GP5 - General planning considerations.  
Policy BD2 – New buildings  
Policy BD4 - All mechanical plant  
Policy BD5 – Residential amenity  
Policy LD1 - Landscape design

8.4 **The Site Allocations Plan** was adopted in July 2019 elements of this were remitted and subsequently adopted in January 2024.

8.4.1 The site and neighboring sites lie within the designated city centre, which are also identified in the Site Allocations Plan (SAP) as a Mixed Use Allocation site ref. MX1-11 for at least 50380sqm office use and 463 residential units.

8.5 The Natural Resources and Waste Local Plan was adopted by Leeds City Council on 16th January 2013 with revised policies Minerals 13 and 14 (Adopted September 2015). The plan sets out where land is needed to enable the City to manage resources, like trees, minerals, energy, waste and water over the next 15 years, and identifies specific actions which will help use natural resources in a more efficient way. Relevant policies include:

GENERAL POLICY1 – Presumption in favour of sustainable development.  
AIR1 – The Management of Air Quality through Development measures.  
WATER1 – Water efficiency  
WATER2 – Protection of Water Quality  
WATER7 – No increase in surface water run-off, incorporate SUDs.  
LAND1 – Land contamination to be dealt with.<sup>1</sup>  
LAND2 – Development conserve trees and introduce new tree planting.

8.6 The National Planning Policy Framework (NPPF) was updated in December 2023 and sets out the Government's planning policies for England and how these should be applied (para 1), and is a material consideration in planning decisions (para 2). It states that the purpose of the planning system is to contribute to the achievement of sustainable development (para 7). So that sustainable development is pursued in a positive way at the heart of the Framework is a presumption in favour of sustainable development (paras 10-11). It states that decision makers at every level should seek to approve applications for sustainable development where possible (para 38). The Framework sets policies on the following issues which are relevant to this planning application proposal (including section numbers):

2 Achieving sustainable development (paras 7, 8, 10, 11, 12)  
4 Decision making (paras 38, 39, 40, 41,42, 43, 47, 48, 55, 56, 57, 58)  
6 Building a strong competitive economy (para 85)  
7 Ensuring the vitality of town centres (paras 90, 91)  
8 Promoting healthy and safe communities (paras 96, 97, 98,101, 104)  
9 Promoting sustainable transport (paras 108,109,112, 115,116,117)

- 11 Making effective use of land (paras 123, 124, 128)
- 12 Achieving well designed places (paras 131, 132,133,135, 136,140)
- 14 Meeting the challenge of climate change and flooding (paras 157-175)
- 15 Conserving and enhancing the natural environment (including ground conditions (paras 180, 185, 186, 188, 189,190,191,192,194)
- 16 Conserving and enhancing the historic environment (paras 200, 201,203)

## 8.7 Relevant Supplementary Planning Guidance includes:

- SPD Building for Tomorrow Today: Sustainable Design and Construction
- SPD Transport
- SPD Accessible Leeds
- SPD Tall Buildings Design Guide
- SPG City Centre Urban Design Strategy
- SPG Sustainable Drainage in Leeds

## 9.0 MAIN ISSUES

- Principle of the proposed use
- The impact on the character and appearance of the street scene and wider area
- Impact on future residential amenity
- Landscape proposals
- Biodiversity
- Inclusivity
- Sustainability and Climate Change
- Wind impact
- Highways and transportation considerations
- Air Quality/ Noise
- Safety and security
- Other matters
- S106 legal agreement
- Planning Balance

### Principle of the proposed use

- 10.1 The site and wider area is allocated in the Site Allocations Plan (SAP) under site reference MX1-11 for office and residential uses, with a site capacity of 50,380sqm office development and 463 residential units.
- 10.2 This proposal consists of an office use (Class E(g)(i)) and includes an area for a café (Class E(b)) and co working space (Class E(g)(i)) (251m<sup>2</sup>) which will be open to the public at ground floor. The café/co working space would activate the ground floor frontages, generate footfall, provide vibrancy to the development, and serve nearby residents and other business users in this part of the city. The space would be limited in floorspace and on this basis is not considered to undermine the vitality of the prime shopping area within the city centre; providing a direct and targeted element to support the proposed use in the scheme and vicinity of it. Control of this matter will be addressed by condition which will also serve to prevent the future introduction of uses which could be detrimental to the amenities of the site without further consideration within Class E and may (as a result of the nature of such uses) promote a deadened frontage to the public spaces and/or may have different servicing needs which require further consideration.
- 10.3 The development has been indicated to provide (approximately) 19,236sqm of office accommodation and would involve the regeneration of a site which has laid dormant for

a number of years and now falls within an area of ongoing regeneration. The site falls within the designated city centre and it is considered that the proposed office use would contribute to the ongoing mix of a vibrant community in this area of the City. It is acknowledged that this proposal would not feature any residential accommodation, however residential uses have already been provided to the north of the site which fall within the wider allocation.

#### The impact on the character and appearance of the street scene and wider area

- 10.4 The proposal seeks to create a 12 storey office building of which the height of the building has been carefully considered and is based on the surrounding context to this area of the city centre. This consideration has also taken into account potential impact on the setting and key views of nearby heritage assets to the east and from within the nearby Holbeck Conservation Area to the south/east of the site. It is considered that the proposed scale of the building would form part of the existing modern character of buildings along the north side of this stretch of Whitehall Road as viewed from within the more historic conservation area and would not impact upon existing heritage assets of which the existing developments to the wider site would shield this building from any views from historic assets. Given this, the buildings height would not unduly dominate the skyline and would be contextual to similar developments within this side of the city centre.
- 10.5 In terms of the urban grain, the footprint of the building remains broadly in the same position as to that approved previously at outline stage and then subsequently approved at reserved matters albeit with alterations to the design in order secure the desired sustainability credentials. The proposal compares favourably to the character of the surrounding street scape. Whilst the site is currently an open cleared area of land, the existing site is uncharacteristic when considering the emerging urban grain of surrounding developments to the north and east. This development would create an infill to existing brownfield land and a new arrangement of building with enhanced pedestrian routes to the west and vehicular access which is intended to sit within the existing street pattern.
- 10.6 The elevational arrangement to this building is highly ordered with regular grid arrangements of brick and different shades, with windows set into reveals with balconies to the northern and southern elevations. The final materials of all built elements will be controlled through conditions which will allow for a finer degree of control. The proposals would be of a high quality design, with detailing and materials that would be appropriate to the wider context, complementing the character and appearance of the surrounding area.

#### Impact on future residential amenity

- 10.7 Neighbourhoods for Living (NFL) provides general guidance on traditional minimum distances when based in a suburban area, which range from 21m for main living uses to other main living uses and 15m from secondary uses to secondary uses. The explanatory text within NFL also states that these distances are a guide and do not take into account the local context. Given the site is located within Leeds City Centre boundary and the existing tight urban grain, the traditional 21m and 15m separation distances which are applicable to suburban sites would not be appropriate on this site given the context and density of the neighbouring area. There is no specific guidance on relationships between buildings in the city centre and this needs to be based on a contextual approach.

- 10.8 The separation distances of the building from the emerging neighbouring buildings have been assessed. The height of this building (12 storeys) is a lot smaller than the residential blocks (Purple B) consented to the north (17 & 21 storeys). This building would retain at least 14.8m to bedroom uses and 16.4m to main living uses within block b of Purple B to the north, and 23m to the east to the neighbouring office building, 22m would be retained to the Get Living development to the south, and 17m towards the railway embankment at the closest point to the west. These separation distances between commercial uses and residential uses are considered commensurate with the existing and emerging context of the surrounding area of the city centre.

#### Landscape proposals

- 10.9 A variety of planting will be involved, including environment and setting appropriate species and specimen sizes of trees, shrubs, grasses, wildflowers and border plants, to ensure an attractive, colourful landscape scheme is achieved as part of the development.
- 10.10 As part of the proposal improvements to Whitehall Road would consist of a designated cycle path with bus bypass along with new landscaping to the north, east, south and west of the building. A minimum 2m footway along the eastern and northern side of the site will also be accommodated.
- 10.11 The landscaping proposed to the north consists of shade tolerant planting which will feature climbing plants against the aluminium perforated cladding to the ground floor. The planting to the eastern boundary of the site would create a buffer between the proposed development and Whitehall and would consist of a 1.8m wide pedestrian route, with full shade/ partial shade tolerant planting beds in between with seating for people to dwell and a pedestrian route of 2m wide along the buildings perimeter.
- 10.12 The main access into the building is from the south in which landscaping would consist of tree planting with a mixture of full sun and shade tolerant planting along the frontage to soften the buildings appearance. The planting would also sit alongside the councils improvements works along Whitehall Road, including a bidirectional cycling lane, footway improvements and bus stop bypass.
- 10.13 Vehicular access into the site is from Whitehall and leads into the undercroft car park to the west of the building. Beyond the car park an area of landscaping is proposed which will feature tree planting, areas of seating and an improved north/south pedestrian link and planting of both shade tolerant and full sun tolerant planting, depending on the location.
- 10.14 Comments raised by Members at the 10<sup>th</sup> August 2023 pre application presentation related to the heritage of the site and how this could be incorporated into the final scheme. In response to these comments, the applicant has confirmed that the approach to the site will consider the historical use of the site and will draw inspiration and reference to the masonry firing hearths, the cart tracks and iron Ingot through the shapes and areas proposed within the hard landscaped area. These features are to be located within the western landscaped area and details of the interpretation including signs about the history of the site will be conditioned as part of any approval.
- 10.15 The roof terrace of the building creates a private space for employees and a breakout space. The terrace would feature a green wall with areas of landscaping within planting beds providing opportunities for recreation, gathering, socialising and potential group activities.

- 10.16 The soft landscaping proposals will enhance the building base and the external green environment with the inclusion of low level planting which will provide a green carpet supported by additional planting specifically chosen to mature in shaded settings. The perforated screens at ground and first floor levels are softened with climber plants, adding interest and biodiversity immediately at street level.
- 10.17 The areas of landscaping will be managed, and the emerging design and layout offer natural surveillance of the external landscaped areas. Full details of all soft and hard landscape proposals and their maintenance will be required to come forward under planning conditions.

### Biodiversity

- 10.18 Core Strategy policy G9 states that developments will need to demonstrate: (i) That there will be an overall net gain for biodiversity commensurate with the scale of the development, including a positive contribution to the habitat network through habitat protection, creation and enhancement, and (ii) The design of new development, including landscape, enhances existing wildlife habitats and provides new areas and opportunities for wildlife, and (iii) That there is no significant adverse impact on the integrity and connectivity of the Leeds Habitat Network.
- 10.19 With regards to Biodiversity, it is noted that the Baseline is 1.33 Habitat Biodiversity Units (lost) and that through landscaping there should be approx. 0.91 Habitat Biodiversity Units delivered on-site. Given this the scheme does not achieve a measurable Biodiversity Net Gain (BNG). To achieve Leeds City Council's aims of a 10% BNG which is soon to be mandated through the section 90A of the Town and Country Planning Act 1990 as amended by the Environment Act 2021. Leeds City Council requires 0.91 Habitat Units. The BNG Assessment states the developer intends to secure these units on Leeds City Council land. In accordance with Leeds City Council guidance, the cost of one Habitat Unit would be £25,000 and would be used towards biodiversity projects/measures which would be delivered in the same ward (or immediately adjacent to the ward subject to agreement of the Local Planning Authority).
- 10.20 Given the above the proposal is acceptable to the Nature Team subject to the signing of the S106 agreement and the insertion of conditions controlling details of the biodiversity enhancement measures on site.

### Inclusivity

- 10.21 The Equality Act 2010 requires local authorities to comply with the Public Sector Equality Duty. Taking into account all known factors and considerations, the requirement to consider, and have due regard to, the needs of diverse groups to eliminate discrimination, advance equality of opportunity and access, and foster good relations between different groups in the community has been fully taken into account in the consideration of the planning application to date and at the time of making the recommendation in this report.
- 10.22 In this instance it is considered that the proposals do not raise any specific implications in these respects and therefore it is not considered that a full Equality, Diversity, Cohesion and Integration Impact Assessment (EDCI) is required.
- 10.23 The building would feature fully accessible entrances and lift access would be provided to all floors. The developer has confirmed the development would provide accessible accommodation in line with the Accessible Leeds SPD, BS8300 and Building



Regulations standards and on this basis, subject to a condition covering this the proposal is considered acceptable and would comply with The Equality Act 2010.

### Sustainability and Climate Change

- 10.24 Members will be aware that the Council has declared a Climate Emergency. Existing planning policies seek to address the issue of climate change by ensuring that development proposals incorporate measures to reduce the impact of non-renewable resources.
- 10.25 The proposal will introduce a number of measures to ensure that Core Strategy policy EN1 (Climate Change – Carbon Dioxide Reduction) is complied with. The developer's accompanying sustainability statement confirms that reduction in energy use is achieved through Air Source Heat Pumps (ASHPs) and Photovoltaics (PV) which are to be implemented into the design of this development:
- 10.26 The above measures will ensure a minimum of 20% reduction in carbon dioxide emissions is achieved (23%) against the Building Regulations Target Emission Rate Part L 2021 and energy reduction through the use of renewable energy generation would meet the 10% (59%) figure as set out in policy EN1.
- 10.27 In addition to the above, early evidence supplied has stated that the development will achieve a BREEAM standard of 'outstanding' which exceeds the requirement of Policy EN2, which requires an BREEAM standard of 'excellent'. To ensure this The Design stage BREEAM certification will need to be provided within 3 months of commencement of construction and the final BREEAM certification should be provided within 12 months of occupation. These details will be secured via condition.
- 10.28 In regard Policy EN4, the proposal is currently not located close to the District Heating Network but there are plans to extend the district heat network to the South Bank in the next 3-5 years, and the eventual pipe route would likely run right past this site. Due to the uncertainty of this proposed extension the applicant has agreed to revisit the potential to connecting to the District Heating network at discharge of condition stage. This said, the site layout has now been annotated with a District Heating Room location and the Council's Sustainability Officer has now accepted this position and the control of this matter via condition.
- 10.29 It is further noted that the additional tree planting, improved accessibility and the introduction of electric vehicle charging points within the car parking areas of the site, will also assist in tackling climate change and air pollution in line with wider Council objectives and assist in encouraging more sustainable travel choices.

### Wind impact

- 10.30 A wind study was submitted as part of the application which considers the impact on the prevailing winds as a result of the development/proposed developments and any creation of localised wind focused micro-climates. The Lawson distress criteria is an industry standard and as such this has been employed in the wind study. This criteria states that a wind speed greater than 15 metres-per-second occurring for more than two hours per year is classified as unsuitable for the general public and represents a wind speed with the potential to destabilise the less able members of the public such as the elderly, cyclists and children. Able-bodied users are those determined to experience distress when the wind speed exceeds 20 metres-per-second for two hours per year.

- 10.31 The wind study has been peer reviewed and it has been demonstrated that there are wind safety exceedances present across the Latitude Yellow site and neighbouring Purple B and Red sites, in both the current and cumulative surrounds scenarios. The most significant of these are to the SW of Purple B where exceedances are found at more than 10 locations, with durations of up to 26 hrs/yr, against a Lawson threshold of 2 hrs/yr. The modellers have shown that construction of the proposed Latitude Yellow building, including its “builtin” wind mitigation measures (screens, canopies etc.) and one offsite screen adjacent to the Get Living development (19/03109/FU), is effective in removing most of these wind safety issues, in both the existing and cumulative surrounds scenarios, and in significantly reducing the size and duration of the exceedances that it cannot remove.
- 10.32 The risk to pedestrians on the Latitude Purple B site from these residual wind exceedances (i.e. post construction of Latitude Yellow) is classed as acceptable by the independent peer reviewer due to the limited size and duration of the hazard and the conservative approach to the wind modelling work which does not take into account the likely mitigation effect of the landscaping features approved on the under construction Latitude Purple B site. Results were also produced for the previous, consented design of the Latitude Yellow building, which demonstrated that wind conditions are better with the newly proposed scheme than they are in the consented scheme.
- 10.33 The wind study has been peer reviewed and is considered to be robust and of a suitable quality and to provide realistic result, consistent with the reviewer’s expectations and no recommendations are requested.

#### Highways and Transportation Considerations

- 10.34 The site is located in a highly sustainable city centre location. The proposal includes 10 car parking spaces (including 2 disabled spaces). In addition, 5 motorcycle parking spaces are also proposed. Given the sustainable location of the development, this provision is acceptable and raises no concerns.
- 10.35 The applicant proposes Electric Vehicle Charging Points (EVCP) to all car parking spaces in line with the standards set out within CS Policy EN8. The specification for the EVCPs will be secured via planning condition.
- 10.36 The development would also provide 138 long stay cycling spaces for employees which will be split between the ground floor level and mezzanine level and comprise of Sheffield Stands, Double Racks and Brompton Lockers. Confirmation has also been provided regarding how cyclists will access the mezzanine level, which will be via an enlarged 1.2m wide staircase with a wheel ramp. In addition, there would also be 20 short stay spaces provided through 10 Sheffield stands within the landscaped area, close to the main entrance which will be provided with natural surveillance.
- 10.37 Servicing and deliveries are proposed to operate via the access road to the east, which also serves the other developments. Final details of this will be secured via conditions for a car park, servicing, and delivery management plan.
- 10.38 The scheme also incorporates a new bidirectional cycleway to the north side of Whitehall Road, including a bus stop bypass. This cycleway will link into the wider cycle improvement scheme by The Council which is proposed along Whitehall Road.
- 10.39 In the scenario that this development is built along with the consented Get Living scheme (19/03109/FU) two wind safety exceedances are created within the carriageway of Whitehall Road. Whilst the risk to pedestrians could be reduced by

introducing guard rails in that vicinity to prevent pedestrians from crossing the highway, the Local Planning Authority (LPA) wouldn't be able to prevent cyclists from using the area of exceedance, especially if they were overtaking a bus at the stop. Given this a wind mitigation screen has been proposed to the southern side of Whitehall Road, which removes these exceedances. The screen would be 25m long with 20m located at ground level and then 5m located 2.6m above ground level. At the time of finalising this report, a meeting has been scheduled with highways and planning to discuss the acceptability of this screen within the adopted highway and in the event that 19/03109/FU is consented. Members will be updated verbally on this element and this discussion does not affect the overall proposal which is to be presented to Members.

- 10.40 As a result of this development, there will be a direct impact on the Globe Road/Whitehall Road Junction. There is an identified scheme to improve the Globe Road/Whitehall Road Junction, forming part of the city centre Package, therefore a contribution is required towards this scheme, commensurate with the size of the development. The cost of the junction works are conservatively estimated at £1.365M. Taking contributions from other developments into account, the contribution required is £188,250.00. This contribution has been agreed to and will be secured via the legal agreement.
- 10.41 In summary, the proposal would have an appropriate level of parking, make appropriate transportation provision, promote sustainable travel and improved connectivity and would deliver cycle improvements along Whitehall Road and improved pedestrian links to Whitehall, through a new public realm space, and would not be likely to give rise to adverse parking, road safety or amenity concerns.

#### Air Quality/ Noise

- 10.42 An Air Quality Assessment has been carried out by the applicant which confirms impact on air quality standards would be negligible at the site or elsewhere as a result of the proposed development.
- 10.43 During the determination of the application a Noise Assessment has been submitted which Environmental Health have been consulted on and agree with their methodology and criteria adopted. Noise limits for new fixed plant are also acceptable and it is recommended that final details of plant are also secured via condition. In addition, a further condition is also recommended limiting construction hours from 0800-1800 Monday to Friday and 0800 to 1400 on Saturdays with no working on Sundays or Bank Holidays. In addition, a condition is also recommended restricting the use of the external roof top area after 11pm, 7 days a week, given the evolving character to this area of the city and to protect future residential amenity.

#### Safety and Security

- 10.44 The Applicant has provided the following statement on safety and security; *The building has been designed to be as passively secure as possible. The ground floor layout affords good levels of natural surveillance in and around primary building access points. These are supported by full site CCTV coverage ensuring that anti-social behaviours or loitering are discouraged. Planted greenery around the building is kept purposefully low so as not to hinder surveillance. The main reception access doors will permit free access into a reception space which will be monitored. Other access points will be equipped with suitable alarm detection and CCTV coverage. A dedicated building manager's room will provide a place for continuous building security monitoring. The building structure and low-level planting structures could be considered to provide a degree of protection against vehicle ramming. Further work*

*may be required to develop this strategy. The building also proposes to follow the principles of secured by design without certification. Ground floor glazing and door specification will require to provide suitable and appropriate levels of crime protection. It is not envisaged that an external party would rent or occupy the level 12 terrace space. Inviting un-solicited persons into the building and onto this level may be seen as conflicting with the rest of the building's safety and security strategy.*

- 10.45 To ensure the above measures are implemented conditions are recommended securing details of all security measures and external lighting strategy. It is considered that the activation of this site would have a benefit to the people using this site/walking by. The development will provide active frontages at ground floor and an increase in pedestrian movements through and around the site, therefore a greater amount of natural surveillance and lighting, when compared to the open land site as it stands today.

#### Other matters

- 10.46 In respect of drainage matters, the application site lies within Flood Zone 1 and there have been no records of any recent flooding within the site or adjacent areas with a low probability to flood. The development has been accompanied with a Drainage Strategy which requires amendments, however given these matters are technical Yorkshire Water and LCC Flood Risk Management are comfortable that these matters can be dealt with via conditions.
- 10.47 Regarding land contamination matters, a site investigation report has recently been submitted which provides additional soil and soil leachate analysis and assessment, ground gas monitoring and assessment. It also confirms that gas protection including for vapours is required, and a cover layer in areas of soft landscape. This report also notes that remedial measures will be finalised by designers and as such doesn't include a soil import strategy or ground gas protection verification plan. Given this the following will be required to be submitted prior to development commencing and will be secured via condition, a soil import strategy and a ground gas protection verification plan.

#### S106 legal agreement

- 10.48 Policy ID2 of the Core Strategy (as amended) is regarding planning obligations and developer contributions, which states that section 106 planning obligations will be required as part of a planning permission where this is necessary, directly related to the development, and reasonably related in scale and kind in order to make a specific development acceptable and where a planning condition would not be effective. This is further enforced by the Community Infrastructure Levy Regulations 2010. which provide that a planning obligation may only constitute a reason for granting planning permission for the development if the obligation is -

- (a) Necessary to make the development acceptable in planning terms,
- (b) Directly related to the development; and
- (c) Fairly and reasonably related in scale and kind to the development.

- 10.49 The Council's adopted policies would result in a requirement for the following obligations;

- 0.91 Biodiversity Units to be Habitat Units delivered off site in the same locality – £25,000
- On site Public Realm areas are publicly accessible

- Travel Plan review fee – £5504
- Contribution for free trial membership and usage of the car club by occupiers of the development and/or other sustainable travel measures for the employees e.g. public transport tickets, hire bikes – £22,000
- Globe Road/Whitehall Road Junction improvements – £188,250
- Management Plan to cover maintenance of Whitehall
- Wayfinding – £16,000
- Employment and training of local people
- Monitoring fees.

10.50 The development is Community Infrastructure Levy (CIL) liable and is estimated to be in the region of £921,368.92. CIL is generally payable on the commencement of development. The CIL contributions are spent on infrastructure projects to meet the needs created by new growth.

## **PLANNING BALANCE**

10.51 The scale and massing of the development is considered wholly appropriate within this dense city centre environment. The identification of this site and wider area within the Site Allocations Plan (SAP) recognised this area of the city for development. The proposed development follows similar principles already established as part of the original Masterplan of this site, which were originally approved in 2006 and have been implemented.

10.52 The proposal would help to further transform a key highly prominent brownfield city centre site, which will complete the final piece in this allocated site. The proposal would also help to regenerate this area of the city in a highly sustainable location and improve/enhance landscaping & connectivity within the city centre and also provide additional facilities to nearby neighbourhoods. The development would also propose all S106 obligations and these matters are given substantial weight in favour of the development.

## **11.0 CONCLUSION**

11.1 In conclusion, the development would involve the further regeneration of a key brownfield allocated site within the city centre boundary. The site is in a highly sustainable location, 20-minute walk to Leeds train station and within easy access to numerous bus stops. The development of this site would also contribute to the mix of developments within this area of the city centre and would also introduce new employment, public landscaped areas, improved pedestrian and cycle connections and add to the vibrancy and vitality to the area and further the regeneration to this part of the city centre. The development of this site would also follow similar design principles already established as part of the overall Masterplan for this site, approved in 2006 originally.

11.2 Therefore, on balance the proposal is considered to be in accordance with the Development Plan and is considered to be acceptable and is therefore recommended for approval, subject to the conditions set out in Appendix 1 and the signing of a legal agreement to secure the obligations listed in paragraph 10.49.

### **Background Papers:**

23/06266/FU

PREAPP/22/00095

## Appendix 1

- 1) The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Imposed pursuant to the provisions of Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

- 2) The development hereby permitted shall be carried out in accordance with the approved plans listed in the Plans and Specifications above.

For the avoidance of doubt and in the interests of proper planning.

- 3) Prior to the installation of any external facing material to the proposed building, full details including a sample panel of the relevant external facing materials, roofing and full details of glazing types shall be constructed on-site and approved in writing by the Local

Planning Authority. The external facing materials, roofing and glazing materials shall be constructed in strict accordance with the sample panel. The sample panel shall not be demolished prior to the completion of the development, unless otherwise agreed in writing by the Local Planning Authority.

In the interests of visual amenity.

- 4) Prior to the construction of the following elements of the proposed building, full 1 to 20 scale working drawing details of the following shall be submitted to and approved in writing by the Local Planning Authority:
- a. soffit, roof line, eaves and any external plant area treatments.
  - b. junctions between materials.
  - c. each type of window bay proposed with a minimum reveal depth of 215mm.
  - d. ground floor frontages.
  - e. ventilation grilles

Development shall then be undertaken in accordance with the approved details.

In the interests of visual amenity.

- 5) Prior to first occupation details of both hard and soft landscaping works, including an implementation programme, shall be submitted to and approved in writing by the Local Planning Authority. Details shall include dimensions, proposed finished levels/gradients and sections/elevations identifying construction standards, material dimensions/types, preservative treatments, below ground foundations, colours, textures etc. associated with.

Hard landscape works shall include:

- (a) proposed finished levels and/or contours,
- (b) boundary details, means of enclosure and retaining structures,
- (c) road/pedestrian/cycling paving areas, steps and ramps
- (d) minor artefacts and structures (eg, tree pits in hard paving, raised planting beds, flush planting beds, refuse/cycle storage units, screening, play features, interpretation features, bins, bollards, lights, paved service covers, linear drainage covers, etc.)
- (e) proposed and existing functional services above and below ground (eg. drainage, sewers, power cables, communication cables, pipelines etc., indicating lines, manholes, supports etc.).
- (f) seating (must meet the full accessibility standard set out in British Standard BS8300, and no seating is to include gaps between arm and back rests unless otherwise agreed in writing by the Local Planning Authority).
- (g) details of contrast nosings

Soft landscape works shall include:

- (h) detailed planting plans including the locations of trees, shrubs, grasses, bulbs, hedge planting etc.
- (i) written specifications (including cultivation, stock handling/quality, seasonal/weather restrictions, planting methods, soil amelioration, drainage, irrigation, surface finish and other B.S compliant operations associated with tree, plant and grass establishment);
- (j) schedules of plants noting species, planting sizes and proposed numbers/densities,
- (k) an implementation schedule.
- (l) details of green wall planting

All Soft Landscaping works to be carried out in accordance with Planting checklist (leeds.gov.uk). Hard and Soft works shall be carried out in accordance with the approved details; approved implementation programme and BS 4428:1989 Code of Practice for General Landscape Operations. The developer shall complete the approved landscaping works and confirm this in writing to the Local Planning Authority prior to the date agreed in the implementation programme.

To ensure the provision and establishment of acceptable landscaping.

- 6) Hard and soft landscaping works shall be fully carried out in accordance with the approved details prior to the occupation of any part of the development in accordance with the programme agreed with the Local Planning Authority and to a reasonable standard in accordance with the relevant provisions of appropriate British Standards or other recognised codes of good practice.

To ensure the provision, establishment and maintenance to a reasonable standard of landscaping in accordance with the approved proposals.

- 7) If within a period of five years from the date of planting, any tree/hedge/shrub dies or is removed, uprooted or destroyed, another tree/hedge/shrub of the same species and size as that originally planted shall be planted in the same location as soon as reasonably possible and no later than the first available planting season, unless otherwise agreed in writing by the Local Planning Authority.

To ensure maintenance of a healthy landscape scheme,

- 8) A LANDSCAPE MANAGEMENT PLAN that applies for the lifetime of the development. The first five years should be detailed more fully to cover an extended defects period for the successful establishment of the proposed landscape. The report should include long term design objectives, management responsibilities and maintenance schedules shall be submitted to and approved in writing by the Local Planning Authority prior to the occupation of the development.

The Management Plan must be a standalone, self-contained document that is not reliant on cross referencing additional information/site plans. The document must therefore include copies of all approved landscape plans and specifications as appendices. All in accordance with Planning Guidance No. 2 LANDSCAPE MANAGEMENT PLANS. Ref: <https://www.leeds.gov.uk/docs/Landscape%20management%20plans%20guidance.pdf>. The landscape management plan shall be submitted to, and approved in writing, prior to the occupation of the development and carried out as approved.

To ensure successful aftercare of landscaping.

- 9) Where trees and shrubs are to be planted adjacent to the railway boundary, they should be positioned at a minimum distance greater than their height at maturity from the boundary. Certain broad leaf deciduous species should not be planted adjacent to the railway boundary. Any hedge planted adjacent to the railway boundary fencing for screening purposes should be placed so that when fully grown it does not damage the fencing, provide a means of scaling it, or prevent Network Rail from maintaining its boundary fencing. Below is a list of species that are acceptable and unacceptable for planting in proximity to the railway boundary;

Acceptable: Birch (Betula), Crab Apple (Malus Sylvestris), Field Maple (Acer Campestre), Bird Cherry (Prunus Padus), Wild Pear (Pyrus Communis), Fir Trees - Pines



(Pinus), Hawthorn (Cretaeagus), Mountain Ash - Whitebeams (Sorbus), False Acacia (Robinia), Willow Shrubs (Shrubby Salix), Thuja Plicatat "Zebrina"

Not Acceptable: Acer (Acer pseudoplatanus), Aspen - Poplar (Populus), Small-leaved Lime (Tilia Cordata), Sycamore - Norway Maple (Acer), Horse Chestnut (Aesculus Hippocastanum), Sweet Chestnut (Castanea Sativa), Ash (Fraxinus excelsior), Black poplar (Populus nigra var, betulifolia), Lomba

In order to safeguard the safety and operational effectiveness of the railway

- 10) Prior to the installation of external lighting adjacent to the railway line details of the lighting should be submitted to and approved in writing by the Local Planning Authority following consultation with Network Rail.

To safeguard the safety and efficiency of the railway

- 11) No works to or removal of hedgerows, trees or shrubs, or built structures with birdnesting potential shall take place between 1st March and 31st August inclusive, unless a competent ecologist has undertaken a careful, detailed check of vegetation or built structures for active birds' nests immediately before (within 24 hours) the works commence and provided written confirmation that no birds will be harmed and/or that there are appropriate measures in place to protect nesting bird interest on site. Any such written confirmation should be submitted to the local planning authority within 3 working days of such works commencing.

To protect nesting birds in vegetation and built structures in accordance with the Wildlife & Countryside Act 1981 (as amended) and BS 42020:2013.

- 12) Prior to commencement of development a Protected Species: Reasonable Avoidance Measures Method Statement (PS:RAMMS) for Bats shall be produced by an appropriately qualified ecological consultant and submitted to and approved by the local planning authority. The PS:RAMMS will include the following:
  - a) Identification of those areas/features on site that have the potential to support Bats using appropriately scaled maps to show where these areas are.
  - b) Timing of works to avoid harm to Bats
  - c) Working methods to avoid harm to Bats
  - d) Person responsible for implementing the PS:RAMMS
  - e) The times during site clearance and construction when specialist ecologists need to be present on site to oversee works
  - f) The role of a responsible person (Ecological Clerk of Works) and lines of communication

The approved PS:RAMMS will be implemented in full accordance with the approved details.

To provide assurance a legally protected species will not be affected

- 13) Prior to any above ground level works, a Lighting Design Strategy For Bats shall be produced by an appropriately qualified ecological consultant and submitted to and approved in writing by the local planning authority. The Strategy shall:
  - a) Identify those areas/features on site that are particularly sensitive for commuting and foraging bats - using an appropriately scaled map to show where these areas are

b) Show how and where external lighting will be installed (through the provision of appropriate lighting contour plans and technical specifications) so that it can be clearly demonstrated that areas to be lit will not disturb commuting and foraging bats.

All external lighting shall be installed in accordance with the specifications and locations set out in the Strategy, and shall be maintained thereafter in accordance with the Strategy. Under no circumstances should any additional external lighting be installed without prior consent from the local planning authority in the areas identified in the Strategy as "particularly sensitive for roosting, commuting or foraging bats".

To safeguard a protected species (bats).

- 14) Prior to any above ground level works, a Plan shall be submitted to and approved in writing by the local planning authority of: integral bat roosting and integral bird nesting features (for species such as House Sparrow and Swift) within the building. Features that are not integral will only be considered for approval if an appropriately qualified ecologist provides assurance that, following discussions with the building architect, integral features are not possible. The agreed Plan shall show the number, specification of the bird nesting and bat roosting features and where they will be located, together with a timetable for implementation and commitment to being installed under the instruction of an appropriately qualified bat consultant. All approved features shall be installed prior to first occupation of the building and retained in the manner as approved thereafter.

To maintain and enhance biodiversity

- 15) Prior to first occupation of the building, written confirmation of integral bat roosting and/or integral bird nesting features will be submitted and approved in writing by the local planning authority. This should include photographs of features in-situ and a written statement that all features have been installed as per the agreed specifications and locations.

To maintain and enhance biodiversity.

- 16) The disabled parking shown on the approved plans shall be laid out prior to first occupation of the development and retained for the life of the development.

In accordance with the adopted Core Strategy and parking policies.

- 17) Means of vehicular access to and from the site shall only be as shown on the approved plan ref TBC and delivered prior to first occupation and retained thereafter for the lifetime of the proposed development.

To ensure the free and safe use of the highway

- 18) Notwithstanding the approved details, works above the ground floor slab level shall not commence until full details of cycle/motorcycle parking and facilities have been submitted to and approved in writing by the Local Planning Authority. The approved cycle/motorcycle parking and facilities shall be provided prior to first occupation of the development and retained thereafter for the lifetime of the development.

In the interests of highway safety and promoting sustainable travel opportunities.

- 19) Development shall not be occupied until a plan showing details of dedicated space for loading, unloading and parking of service/delivery vehicles has been submitted to and approved in writing by the Local Planning Authority. This space shall be retained for the lifetime of the development.

To ensure the free and safe use of the highway.

- 20) Development shall not be occupied until a Car Park and Servicing Management Plan (including timescales and detailed loading bay proposals) have been submitted to and approved in writing by the Local Planning Authority. The plan shall be fully implemented, and the development thereafter operated in accordance with the approved timescales.

To ensure the free and safe use of the highway.

- 21) Prior to occupation of the development, the off-site highway works as shown on plan IN comprising TBC at location TBC shall be fully delivered.

To ensure the free and safe use of the highway

- 22) Development shall not be occupied until Electric Vehicle Charging Points have been provided in accordance with a scheme that shall have been submitted to and approved in writing by the Local Planning Authority. The approved facilities shall thereafter be retained for the lifetime of the development.

In the interest of promoting low carbon transport

- 23) Development shall not commence until a statement of construction practice has been submitted to and approved in writing by the Local Planning Authority, the statement of construction practice shall include full details of:
- a) the construction vehicle routing, means of access, location of site compound, storage and parking (including workforce parking), means of loading and unloading of all contractors' plant, equipment, materials and vehicles and associated traffic management measures.
  - b) methods to prevent mud, grit and dirt being carried on to the public highway from the development hereby approved.
  - c) measures to control the emissions of dust and dirt during construction.
  - d) how the statement of construction practice will be made publicly available by the developer.

The approved details shall be implemented at the commencement of works on site and shall thereafter be retained and employed until completion of the works on site. The Statement on Construction practice shall be made publicly available for the lifetime of the construction phase of the development in accordance with the approved method of publicity.

The carrying out of the development could result in significant harm to the amenities of local residents and highway safety, and accordingly details of construction practice is required to be agreed prior to commencement of works in order to protect such interests.

- 24) Development shall not commence until a construction methodology has been submitted to and approved in writing by the Local Authority. The construction methodology shall demonstrate consultation with the Asset Protection Project Manager at Network Rail. The development shall thereafter be carried out in accordance with the approved

construction methodology unless otherwise agreed in writing by the Local Planning Authority.

The safety, operational needs and integrity of the railway.

- 25) The hours of construction shall be restricted to 08.00 - 18:00 hours Monday to Saturday, with no works on Sundays or Bank Holidays, unless agreed in writing with the Local Planning Authority.

In the interests of residential amenity.

- 26) Within 24 months of the completion of the installation of external facing materials as hereby approved (such a date as to be notified to the Local Planning Authority ) in the event of any complaint to the Local Planning Authority from Network Rail relating to signal sighting safety or driver distraction, upon notification to the Local Planning Authority , the applicant shall as soon as possible and not later than 28 days, submit for approval to the Local Planning Authority details of a scheme of remedial measures to address the concerns raised with details of a timescale for implementation of the works. The works shall be carried out in accordance with the approved details and timetable.

To ensure safety of the users of the railway.

- 27) The site shall be developed with separate systems of drainage for foul and surface water on and off site, unless otherwise agreed in writing by the Local Planning Authority. The separate systems should extend to the points of discharge to be agreed.

In the interest of satisfactory and sustainable drainage

- 28) There shall be no piped discharge of surface water from the development prior to the completion of surface water drainage works, details of which shall be submitted to and approved by the Local Planning Authority. If discharge to public sewer is proposed, the information shall include, but not be exclusive to:
- i) evidence that other means of surface water drainage have been properly considered and why they have been discounted; and
  - ii) the means of discharging to the public sewer network at a rate not to exceed 3.5 litres per second

To ensure that no surface water discharges take place until proper provision has been made for its disposal

- 29) The approved Phase II Site Investigation report ('Geo-Environmental Development Appraisal' for Latitude Yellow, Woolgar Hunter, 23/11/2023, 20226001 v1.11) indicates that remediation is necessary, and therefore development (excluding demolition) shall not commence until a Remediation Strategy has been submitted to, and approved in writing by, the Local Planning Authority.

The Remediation Strategy shall include a soil import methodology, a ground gas protection verification plan including detailed design of protection measures, a programme for all works, and allow for the provision of Verification Reports. It is strongly recommended that all reports are prepared and approved by a suitably qualified and competent person.

To ensure that the site is 'suitable for use' with respect to land contamination.

- 30) If remediation is unable to proceed in accordance with the approved Remediation Strategy, or where significant unexpected contamination is encountered, or where soil or soil forming material is being imported to site, the Local Planning Authority shall be notified in writing immediately and operations on the affected part of the site shall cease. The affected part of the site shall be agreed with the Local Planning Authority in writing. An amended or new Remediation Strategy and/or Soil Importation Strategy shall be submitted to and approved in writing by the Local Planning Authority prior to any further remediation works which shall thereafter be carried out in accordance with the revised approved Strategy. Prior to the site being brought into use, where significant unexpected contamination is not encountered, the Local Planning Authority shall be notified in writing of such.

It is strongly recommended that all reports are prepared and approved by a suitably qualified and competent person.

To ensure that any necessary remediation works are identified to make the site 'suitable for use' with respect to land contamination.

- 31) Remediation works shall be carried out in accordance with the approved Remediation Strategy. On completion of those works, the Verification Report(s) shall be submitted to the Local Planning Authority in accordance with the approved programme. The site or phase of a site shall not be brought into use until such time as all verification information has been approved in writing by the Local Planning Authority.

It is strongly recommended that all reports are prepared and approved by a suitably qualified and competent person.

To ensure that the remediation works are fully implemented as agreed and the site has been demonstrated to be 'suitable for use' with respect to land contamination.

- 32) Prior to the commencement of any above ground works an updated Sustainability Statement shall be submitted to and approved in writing by the Local Planning Authority, which will include a detailed scheme comprising:

- a) Policy EN1(i) SAP/ SBEM/ BRUKL report
- b) Policy EN1(ii) calculation of the building's energy demand with what percent of it is being generated by low and zero carbon sources. Details of specifications and locations of all Low or Zero Carbon (LZC) energy sources on site are also required.
- c) Policy EN2 BREEAM certification.
- d) Policy EN4 evidence for one of the four policy options needs to be provided.

The development shall be carried out in accordance with the approved details and

- e) Within 12 months of the final occupation a post-construction review statement shall be submitted to the Local Planning Authority including demonstration that the building(s) have achieved the relevant standards. The development and buildings comprised therein shall be maintained thereafter and any repairs shall be carried out all in accordance with the approved detailed scheme and post-completion review statement.

In the interests of ensuring the development meets the requirements of the adopted energy policies within the Core Strategy.

- 33) The commercial ground floor use shall be used as Class E(a) and/or Class E(b) and/or Class E(g)(i) and shall not be used for any other purposes within Class E or other Use Classes of the Schedule of The Town and Country Planning (Use Classes)

(Amendment) (England) Regulations 2020, or in any provision equivalent to that Class in any Statutory Instrument revoking or re-enacting that Order with or without modification.

As other uses may conflict with the requirements of the adopted development plan and have highway safety implications.

34) There shall be:

No speakers for the playing of amplified music or sound shall be installed on the roof terraces and there shall be no use of the commercial roof terrace after 11pm (except for smoking).

To protect residential amenity given the evolving character to this area of the city centre.

35) Prior to the first use of the ground floor cafe/workspace area, details of a signage/window manifestation strategy shall be submitted to and approved in writing by the Local Planning Authority. This shall set out signage zones on the building and shall at no time be solidly obscured or screened to prevent vision into and through the windows

In the interest of ensuring active frontages and natural surveillance are maintained.

36) Notwithstanding the plans hereby approved, prior to installation of the agreed on site wind mitigation measures as referenced in approved plans: PROPOSED WIND MITIGATION - LOW LEVEL ref: LYPRS-CCA-ZZ-XX-D-A-05023 REV P05, PROPOSED WIND MITIGATION - HIGH LEVEL ref: LYPRS-CCA-ZZ-XX-D-A-05024 REV P05 full design details, including dimensions, exact positions, porosity, materials and fixing details of the wind mitigation measures shall be submitted to and approved in writing by the Local Planning Authority. The approved measures shall be erected prior to first occupation of the development and retained for the lifetime of the development.

In the interests of public safety

37) Notwithstanding the plans hereby approved and in the event that the offsite wind mitigation as referenced in approved plan LYPRS-CCA-ZZ-XX-D-A-05023 S5 P05 as "TYPE 5" is required as a result of the construction of 19/03109/FU details shall be submitted to and approved in writing by the Local Planning Authority, which shall include the full design specification, porosity, and material of the wind mitigation. The development shall then be undertaken in accordance with the approved details.

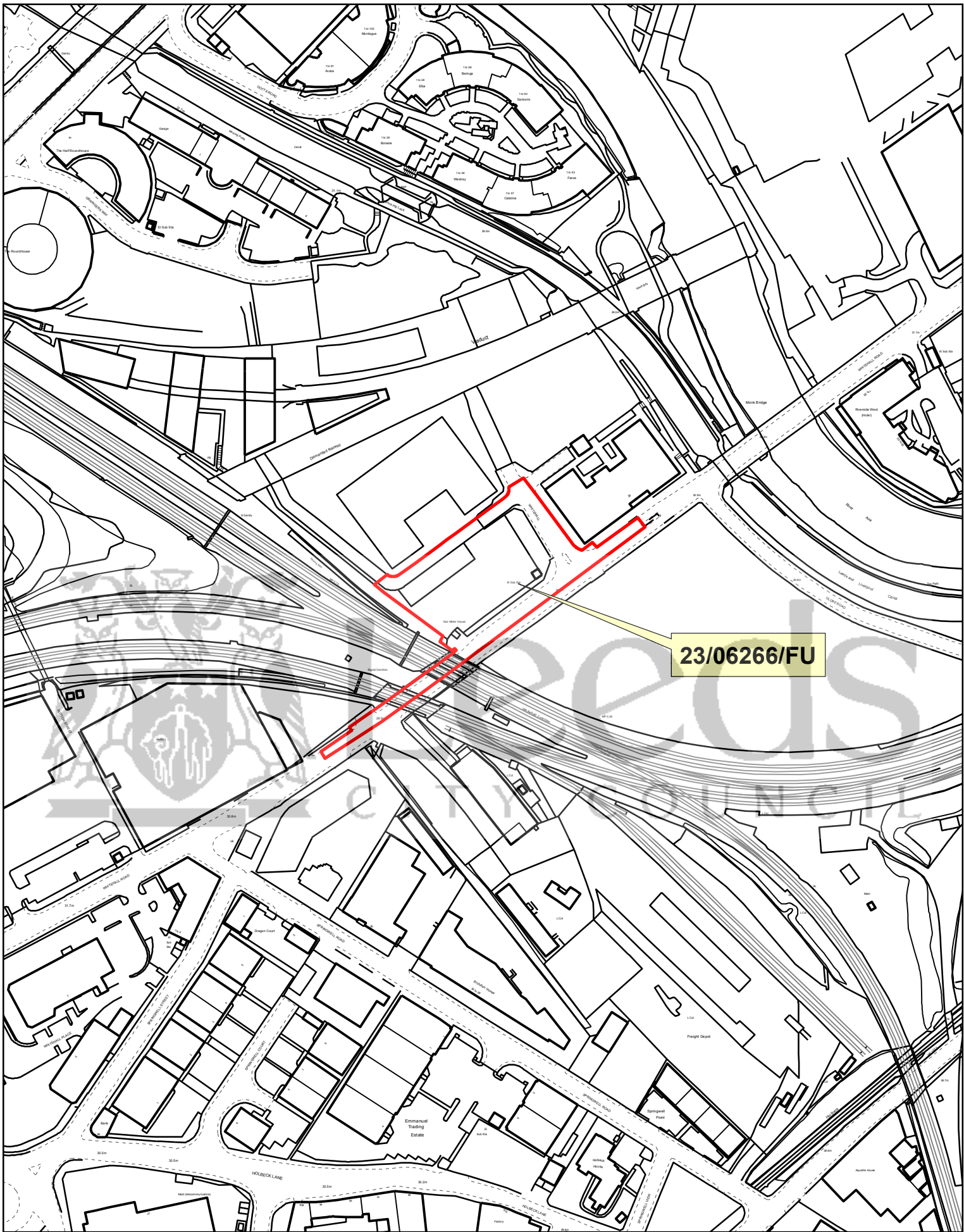
In the interests of public and highway safety

In order to discharge the above condition, the applicant must enter an agreement with the Council under Section 278 of the Highways Act 1980. The applicant is advised to make early contact with the Highway and Transportation Service by emailing the details to [S278Agreements@Leeds.gov.uk](mailto:S278Agreements@Leeds.gov.uk). Further information is available on the council's website at <https://www.leeds.gov.uk/parking-roads-and-travel/licences-and-permits>.

38) Prior to the commencement of construction, a Security Plan listing details of what security measures are proposed shall be submitted and approved in writing by the Local Planning Authority. The Security Plan, which shall be prepared in conjunction with advice from the RSES (Register of Security Engineers and Specialists) shall set out measures to control access to the relevant building; cover and protect public areas, this includes monitored CCTV surveillance to protect the structure and fabric of the building

and the public realms outdoors and internally, including parking for the site both during construction and following completion. The measures thereby approved shall be implemented prior to first use of the development and thereafter retained and maintained.

In the interests of security and public safety.

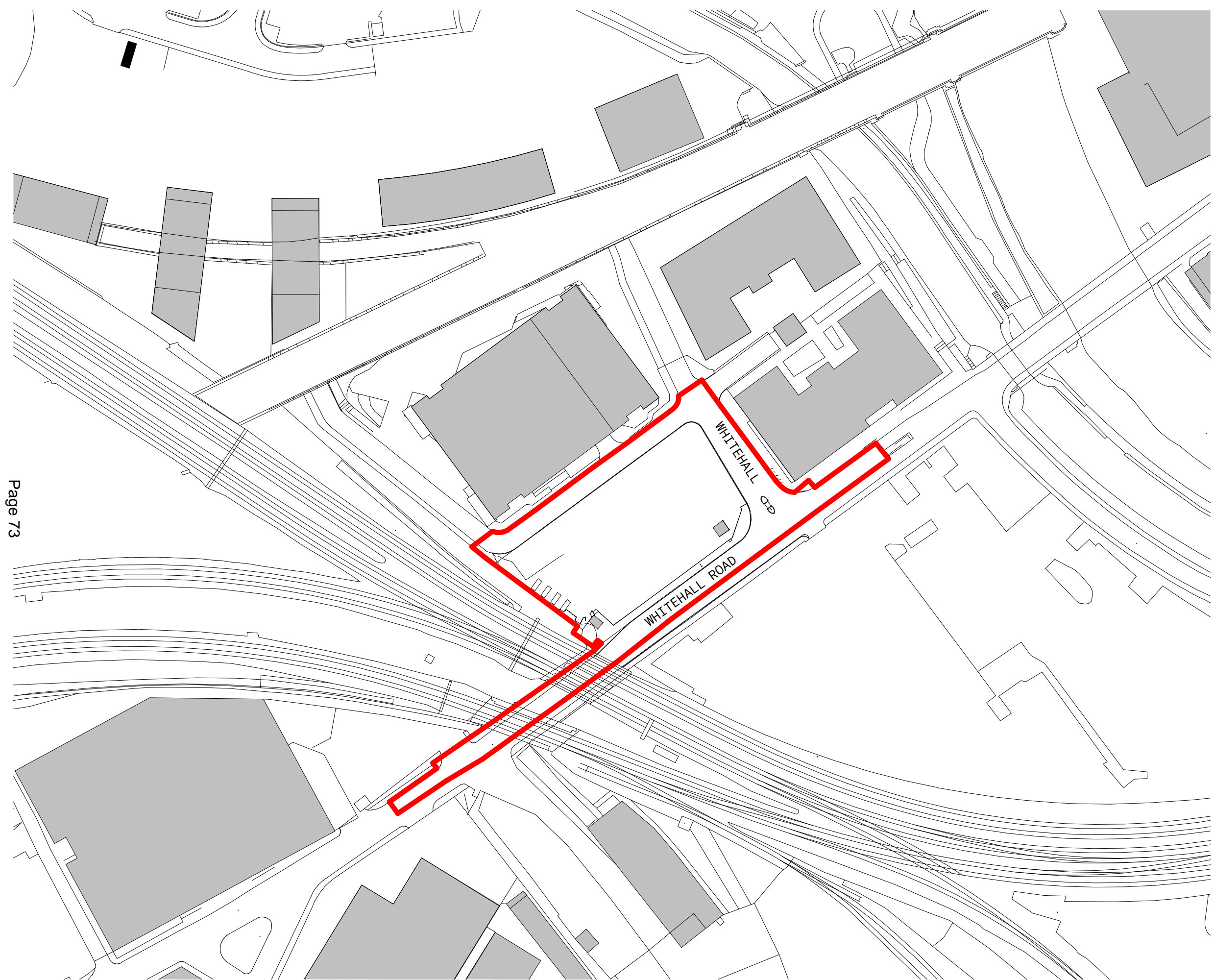


23/06266/FU

# CITY PLANS PANEL







Page 73

The contractor will be held to have examined the site and checked dimensions and levels before commencing construction works.  
 No assumptions should be made without reference to the architect.  
 No dimensions should be scaled from this drawing.

Rev:	Description:	Date:	By:
P01	Planning Issue.	04.10.23	KG
P02	Planning boundary line updated following comments received from LCC Highways Officer.	11.12.23	KG



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Status:  
**PLANNING**

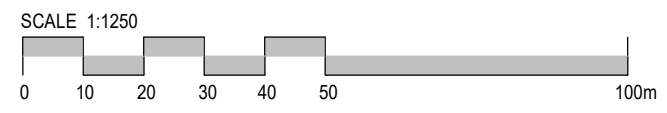
Client:  
 BAM Monk Bridge Ltd

Project Title:  
 Latitude Phase Yellow

Drawing Title:  
 Site Location Plan

Scale:	Date:
As indicated@A3	29.09.23
Drawn by:	Checked by:
KG	DS

Job No:	Drawing No:	Stb	Rev:
21030G	LYPRS-CCA-ZZ-XX-D-A-05001	S5	P02





- NOTES:**
- DO NOT SCALE FROM THIS DRAWING. ALWAYS WORK TO NOTED DIMENSIONS.
  - ALL SETTING OUT, LEVELS AND DIMENSIONS TO BE CHECKED ON SITE PRIOR TO THE INSTALLATION OF MATERIALS AND ANY DISCREPANCIES ARE TO BE BROUGHT TO THE ATTENTION OF THE CONTRACT ADMINISTRATOR IMMEDIATELY.
  - THIS DRAWING MUST BE READ WITH THE RELEVANT SPECIFICATION DOCUMENTS AND DETAIL DRAWINGS. ANY DISCREPANCIES MUST BE BROUGHT TO THE ATTENTION OF THE CONTRACT ADMINISTRATOR IMMEDIATELY.
  - THIS DRAWING IS COPYRIGHT PROTECTED AND MAY NOT BE REPRODUCED IN WHOLE OR PART WITHOUT WRITTEN AUTHORITY.
  - ALL DIMENSIONS ARE IN MILLIMETRES UNLESS OTHERWISE STATED.
  - NOTE DRAWING SUITABILITY CODE SHOWN BELOW. DRAWING NOT TO BE USED FOR CONTRACTUAL AGREEMENT WITHOUT A1 STATUS.

THIS DRAWING WAS PLOTTED ON:  
25/01/2024

REVISION HISTORY

DATE	REV	ZONE	DESCRIPTION	CHK
22/01/2024	P05		Additional Planting	ST
23/01/2024	P06		Updating Cycleway etc	MC
24/01/2024	P07		Graphical change	MC

**GENERAL INFORMATION**

- APPLICATION SITE BOUNDARY**
- HARDWORKS**
  - HIGH QUALITY PAVING TYPE 1**  
SUPPLIER: Tobermore  
PRODUCT: Brosear Ground  
SIZE: 200x100x65mm  
FINISH: Arran Stone  
Bond: Stretcher
  - DETAIL FEATURE BANDING A**  
REF: Exposed Aggregate Insitu Concrete with Creamed Finish  
FINISH: Exposed aggregate  
DEPTH: 120mm to pedestrian areas
  - DETAIL FEATURE BANDING B**  
Supplier: Tobermore  
Product: Reno  
Size: 190 x 60 x 60mm  
Colour: Bracken  
Bond: Herringbone
  - DETAIL FEATURE BANDING C**  
Supplier: Tobermore  
Product: Tegula  
Size: 100 x 100 x 50mm  
Colour: Chocolate  
Bond: Stretcher
  - GREEN GAP PAVING**  
Supplier: Hardscape (O&E)  
Product: Hydro Limeo 40  
Size: 300 x 300 x 100mm  
Colour: Standard Grjs
  - TACTILE PAVING**  
Supplier: Tobermore  
Product: Tactile Flags Natural Hazard Warning  
Size: 400 x 400 x 60mm  
Colour: Natural  
Bond: Stretcher
- EDGING**
  - CONCRETE KERB - Lead flush**  
SUPPLIER: TOBERMORE  
REF: FUSION KERB  
DIMENSIONS: 145x25x95mm  
MATERIAL/FINISH: CONCRETE/TEXTURED  
COLOUR: SILVER GREY
  - CONCRETE KERB - Transitional**  
SUPPLIER: TOBERMORE  
REF: FUSION KERB  
DIMENSIONS: 145x25x95mm  
MATERIAL/FINISH: CONCRETE/TEXTURED  
COLOUR: SILVER GREY
- FURNITURE**
  - PROPRIETARY BENCH**  
SUPPLIER: Logic (O&E)  
REF: Platanu Bench  
MATERIAL/FINISH: UK Grown Sustainable Timber
  - CYCLE STAND**  
SUPPLIER: BROXAP (O&E)  
REF: SHEFFIELD HOOP  
MATERIAL: STAINLESS STEEL  
FRAME: ROOF FIXED
  - CONCRETE STEPS**  
SUPPLIER: Tobermore  
REF: Granite Steps  
Colour: Silver  
DIMENSIONS: 1000 x 150 x 400mm
  - HANDRAIL**  
PRODUCT: TRANS RAILING/FENCE PANELS  
MATERIAL: FLAT BAR STEEL  
ACCESSORIES: SUPPLIED WITH FLAT BAR POSTS  
SUPPLIER: LAND-FULSTON O&E
  - TREE GRILLE**  
SUPPLIER: GREEN-TECH (O&E)  
PRODUCT REFERENCE: FORTRESS TREE GRILLE  
MATERIAL: FINISH STEEL
  - ILLUMINATED BOLLARD**  
For full lighting drawing see 7000 series  
SUPPLIER: STREETLIFE (O&E)  
REF: BRIGHT BOLLARD  
DIMENSIONS: 140 x 140 x 900MM ABOVE GL TBC  
MATERIAL: FINISH WEATHERING STEEL TBC  
DETAILS TBC BY M&E ENGINEER
  - BOLLARD**  
SUPPLIER: STREETLIFE (O&E)  
REF:  
DIMENSIONS: 140 x 140 x 900MM ABOVE GL TBC  
MATERIAL: FINISH WEATHERING STEEL TBC  
DETAILS TBC BY M&E ENGINEER

ORIGINATOR

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Client  
**COOPER CROMAR**

Project  
**LATITUDE LEEDS**

Drawing Title

**GENERAL ARRANGEMENT**

Created By	Checked By	Scale	Sheet Size
MC	ST	1:150	A1
Project Number	Originator	Volume	Level
311	LYR	XX-00-DWG-L-1001	
Work Stage	Status	Revision	
Stage 3	SI For Coordination	P07	



Originator: Matthew Walker  
0113 3788033

## Report of the Chief Planning Officer

### CITY PLANS PANEL

Date: 15<sup>th</sup> February 2024

Subject: 22/04852/FU - Proposed demolition works and erection of 10-storey building to create co-living residential development, Holdforth Court, Brussels Street, Leeds LS9 8AT

Developer: KMRE Group (Holdforth Court) Limited c/o ID Planning, Mr Andrew Windress, 9 York Place, Leeds, LS1 2DS

#### Electoral Wards Affected:

Hunslet and Riverside

Yes

Ward Members Consulted

#### Specific Implications For:

Equality and Diversity

Community Cohesion

Narrowing the Gap

#### RECOMMENDATION:

**DEFER and DELEGATE to the Chief Planning Officer for approval subject to the conditions set out at Appendix A and Section 106 agreement to secure the following:**

**Affordable Housing off site commuted sum (£742,633 tbc and subject to verification by the District Valuer)**

**Offsite highways contribution (£29,400)**

**CAVAT compensation amount for loss of trees (£77,809.00)**

**Residential Travel Plan Fund (£30,662.01) and travel plan review fee of £3815.00.**

**Mechanism to contribute £20,000 to Traffic Regulation Orders as required**

**Biodiversity Net Gain matters to be agreed (see report)**

**Provision of car club space**

**In the circumstances where the Section 106 Agreement has not been completed within 3 months of the resolution to grant planning permission, the final determination of the application shall be delegated to the Chief Planning Officer.**

## **1.0 INTRODUCTION:**

The application site is located on a key and prominent gateway location and the building proposed is of relatively significant scale. The proposal involves a co-living residential use which is an emerging form of residential tenure for the City of Leeds. The proposals are therefore brought to members under subsection G of the exceptions list in the Officer Delegation Agreement where the Chair considers that the application should be referred to the relevant Plans Panel for determination because of the significance, impact or sensitivity of the proposal.

## **2.0 SITE AND SURROUNDINGS:**

- 2.1 The planned location is 1,318 square metres (0.13 hectare) in size and almost triangular in shape. Brick Street to the east, Brussels Street to the north, Crown Point Road to the south, and Duke Street to the west define its boundaries in the South East of Leeds City Centre within the designated City Centre Boundary.
- 2.2 The current site is accessed via Brussels Street, which is an adopted highway which measures approximately 4.1 metres in width within the vicinity of the site and footways on both sides measuring circa two metres in width. The road and footway surfaces are well-maintained and considered to be in an acceptable condition but the building's architectural treatment and lack of active frontage in addition to the unkempt nature of landscaping present a harsh and somewhat unwelcoming presence to both Brick Street and Brussels Street for pedestrians. Opposite the site entrance and spanning the length of Brussels Street are a series of commercial uses housed within railway arches and their respective forecourts. Along Brussels Street, including the approaches to the proposed access and at junctions, there are extensive on-street parking restrictions enforced by double yellow lines and signage warning of 'no stopping at any time'. There are private parking spaces at the western end of Brussels Street which were appointed to the residents who used the site before closure in 2020.
- 2.3 The site sits at a 'crossroads' where the A61 and the viaduct intersect within an emerging residential area around Saxton Gardens and Mill Street. The building currently on site is the vacant four storey St. Anne's Community Service building which is proposed to be demolished. The existing building has a distinctive form characterised by a curved and stepped façade facing south and series of mono-pitched roofs. High sided boundary walling then spans the site boundary to Duke Street beyond which is a belt of mature trees and verge separating the site from the pedestrian and highway environment. The remaining boundaries are protected by a combination of railings and piers and low-level shrub planting and trees. Beyond Brussels Street to the north is the dominating presence of the east-west route Railway Viaduct.
- 2.4 In terms of the wider context, a series of taller buildings within areas identified within the Tall Buildings SPD are either consented or under construction to the east beyond the dominant highway structure of Marsh Lane. The immediate setting is also dominated by the presence of both the Gateway and Ibis hotel buildings due south of the site. Crown Point Bridge forms the key viewpoint of the site on approaches from the south and to the west of Crown Point Bridge is the Conservation Area and Grade I Listed Leeds Minster. The remainder of the land to the west of the site is either occupied by highway, large width central reservation / pedestrian routing to that highway which is landscaped or the parkland area opposite the Minster adjacent to the railway viaduct. Quarry Hill, it's emerging SOYO development and Northern Ballet and Munro House are situated to the north, with a pedestrian route running under the railway viaduct from Brick Street / Brussels Street towards this area.

### **3.0 PROPOSAL:**

#### Building

- 3.1 The proposed 10 storey building comprises common facilities such as laundry facilities, lounges, touchdown workspaces, cinema room, gym, bin / cycle storage and concierge on the ground floor, and 'co-living' studio apartments and shared kitchen and lounge spaces incidental to those apartments, to the upper stories.

Set over a 1,318sq.m (0.13ha) site, the proposed development provides the following:

- Secure cycle store, providing 120 spaces
- 342sq.m communal recreation space at ground floor (level 0)
- 70sq.m communal space on floors 1-7 at each floor
- 40sq.m communal space on floors 8-9 at each floor
- 118 no. 37sq.m (minimum) one person studio apartments  
(Core Strategy Policy H9 Compliant for studio accommodation)

- 3.2 The main pedestrian entrance to the building would be situated on the corner of Brick Street and Brussels Street to provide a legible focal point / entrance and the principal route into the building. This entrance would provide access through the communal area to the main stair and lift core providing access to all floors within the building. A concierge desk and office are to be located within the main entrance lobby to provide a secure access control point, while creating a light filled internal foyer space. From here access is provided to the resident's communal facilities, creating an active frontage onto Brick Street.

- 3.3 The proposed external materials palette includes the use of a dark Red Brick, 'GRP' (Glass Reinforced Plastic) / Natural stone cladding, curtain wall glazing and feature copper panels.

#### Landscaping and planting

- 3.4 The proposals involve the retention of all trees which currently exist beyond the site perimeter and introduction of new street trees to Brick Street in partial mitigation for the loss of seven trees contained within the site boundary. The building has been pulled back from the pedestrian footway on both Brussels Street and Brick Street to improve the pedestrian flow and to reduce the impact of the building at street level. This in turn also helps to preserve two mature trees at the site's northeastern corner which provide amenity value to the street. To the south of the site, an external area with both paving and grass is provided as external amenity area and would be accessed from the communal area at ground floor.

#### Site Access and Parking

- 3.5 Within the ground floor accommodation, the design includes separate secure storage areas for cycles, with direct secure access to outside, as well as an internally accessed bin store allowing external access for servicing from Brussels Street. The scheme is car free given the highly sustainable location of the application site within close travelling distance to the city bus station and short distance to other transport amenities. The site includes a relocation of the existing turning head from Brussels Street to allow for servicing and deliveries.

#### **4.0 RELEVANT PLANNING HISTORY:**

99-20/63/92/FU - 4 storey hostel (approved)

#### **5.0 HISTORY OF NEGOTIATIONS:**

5.1 The application before City Plans Panel was submitted on 02.08.2022 and took the form of an 11 storey residential building comprising a wider variety of unit sizes, comprising 189 units and ground floor external parking. The initially submitted proposals were not considered acceptable to officers on the following basis:

- Poor relationship between non-descript shared amenity spaces and small unit sizes
- Poor elevational treatment and building form
- Failure of the building to respect the character and appearance of the area - building failed to respect the prevalence of Quarry Hill as part of a key gateway location (Tall Buildings Design Guide page 72).
- Poor relationship between the building and the street edges of Brick Street and Brussels Street
- Lack of natural surveillance to surroundings / dead frontages
- Lack of clarity on the day to day operation of the co-living product

A series of design focussed meetings and workshops then took place in late 2022 and early / mid-2023. Revised plans were then received in September 2023 which were then refined in January 2024. It is this latest and final revision series brought forward for consideration by members and determination by City Plans Panel.

#### **6.0 RELEVANT PLANNING POLICIES:**

##### **6.1 Statutory Context**

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires the application to be determined in accordance with the development plan unless material considerations indicate otherwise. For the purposes of decision making at this site, the Development Plan for Leeds currently comprises the following documents:

- The Leeds Core Strategy (Adopted November 2014 and as amended by the Core Strategy Selective Review 2019)
- Saved Leeds Unitary Development Plan Policies (UDPR 2006)
- The Natural Resources & Waste Local Plan (NRWLP 2013) including revised policies Minerals 13 and 14 (2015).
- Leeds Site Allocations Plan (SAP 2019)

These development plan policies are supplemented by supplementary planning guidance and documents.

##### **6.2 Development Plan**

##### **6.3 Leeds Core Strategy (CS)**

Leeds Core Strategy sets out strategic level policies and vision to guide the delivery of development investment decisions and the overall future of the district. The site is located within the City Centre boundary. The most relevant policies are set out below:

- Spatial Policy 1 Location and scale of development.
- Spatial Policy 2 hierarchy of centres and spatial approach to retailing, offices, intensive leisure and culture
- Spatial Policy 3 City Centre Development
- Spatial Policy 6 Housing requirement and allocation of housing land
- Spatial policy 7 distribution of housing land and allocations
- Spatial Policy 8 Economic development priorities
- Spatial Policy 9 Employment
- Spatial Policy 11 Transport infrastructure investment priorities such as pedestrian improvements
- Policy CC1 City Centre Development
- Policy CC3 Improving connectivity between the City Centre and Neighbouring Communities.
- Policy H3 Housing Density
- Policy H4 Housing Mix
- Policy H5 Affordable Housing
- Policy P10 Design
- Policy P11 Heritage
- Policy P12 Landscape
- Policy T1 Transport management
- Policy T2 Accessibility requirements and new development
- Policy H9 Space Standards
- Policy H10 Accessible Dwellings
- Policy EN1 Carbon dioxide reduction
- Policy EN2 Sustainable design and construction
- Policy EN4 District heating
- Policy EN5 Managing flood risk
- Policy EN6 Recycled materials
- Policy EN8 Electrical Vehicle Charging
- Policy G5 Open space provision
- Policy G8 Protection of important species and habitats
- Policy G9 Biodiversity Improvements

#### **6.4 Leeds Unitary Development Plan Review 2006 (UDPR) Saved Policies**

Relevant Saved Policies include:

- Policy GP5 all planning considerations
- Policy BD2 / BD5 design and siting of new buildings
- Policy LD1 landscaping

#### **6.5 Leeds Natural Resources and Waste DPD**

The plan sets out where land is needed to enable the City to manage resources, like minerals, energy, waste and water over the next 15 years, and identifies specific actions which will help use natural resources in a more efficient way.

Relevant policies include:

- Air 1 management of air quality through new development
- Water 1 water efficiency including sustainable drainage
- Water 7 surface water run-off

- Water 2 protection of water quality
- Water 4 development in flood risk areas
- Water 6 flood risk assessments
- Land 1 contaminated land
- Land 2 development and trees
- Minerals 3 coal safeguarding

## **6.6 Relevant Supplementary Planning Documents/Guidance (SPD/SPG):**

- SPD Tall Buildings Design Guide
- SPD Building for Tomorrow Today: Sustainable Design and Construction
- Transport SPD
- SPD Accessible Leeds
- SPG City Centre Urban Design Strategy

### **Other Considerations**

- [Co-Living Technical Guidance Note– May 2023](#)
- HMO, Purpose-Built Student accommodation and Co-Living Amenity Standards SPD (Draft)

## **6.7 Site Allocations Plan**

The site is unallocated within the Site Allocations Plan.

## **6.8 National Planning Policy Framework 2023 (NPPF)**

The NPPF sets out the Government’s planning policies for England and how these should be applied (para 1) and is a material consideration in planning decisions (para 2). It states that the purpose of the planning system is to contribute to the achievement of sustainable development (para 7). So that sustainable development is pursued in a positive way at the heart of the Framework is a presumption in favour of sustainable development (para 11). It states that decision makers at every level should seek to approve applications for sustainable development where possible (para 38).

The Framework sets policies on the following issues which are relevant to this planning application proposal:

- 2 Achieving sustainable development
- 4 Decision making
- 5. delivering a sufficient supply of homes
- 6 Building a strong competitive economy
- 7 Ensuring the vitality of town centres
- 8 Promoting healthy and safe communities
- 9 Promoting sustainable transport
- 11 Making effective use of land
- 12 Achieving well designed places
- 14 Meeting the challenge of climate change and flooding
- 15 Conserving and enhancing the natural environment
- 16 Conserving and enhancing the historic environment



## **6.8 Other Legislation**

Section 66 of the Planning (Listed Building and Conservation Areas) Act 1990 (the “Listed Building Act 1990”) reads:

“In considering whether to grant planning permission... for a development which affects a listed building or its setting, the local planning authority... shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses”

## **6.9 Consultations Undertaken**

### **STATUTORY**

#### **6.9.1 Health and safety Executive (HSE) (Fire Safety – Planning Gateway One)**

Health and Safety Executive have advised that they are content with the submitted proposals in respect of the fire safety strategy for the new building.

#### **6.9.2 Coal Authority**

The Coal Authority have noted the submission of up to date coal mining information for the majority of the site, which lies outside of the Development High Risk Area. The inclusion of a substation facility within the site envelope does however bring the overall development into the Development High Risk Area. The Coal Authority have advised that their information indicates a coal seam is conjectured to outcrop to the west of the site, dipping eastwards beneath the site, which may have been subject to historic unrecorded mining activity. Such workings can pose a risk of ground instability and may give rise to the emission of mine gases.

As such, The Coal Authority consider it appropriate that intrusive investigations should be undertaken to establish the implications posed by possible coal mining legacy to the scheme of development now proposed. This matter will be controlled through a prescribed condition provided by the Coal Authority and will be a pre-commencement requirement.

#### **6.9.3 Yorkshire Water**

Yorkshire Water have advised no objections to the proposals, subject to a condition related to development being undertaken in accordance with the submitted / proposed Flood Risk Assessment for the site.

#### **6.9.4 Natural England**

No comments received in any of the three rounds of consultation undertaken.

#### **6.9.5 Environment Agency**

No comments received in any of the three rounds of consultation undertaken

### **NON-STATUTORY**

#### **6.9.6 Sustainability - Design Team / Conservation Team**

The revised proposal being considered represent the outcome of a series of focussed design workshops. The workshops focussed on meeting the following suggestions to improve over the originally proposed building applied for as part of the application in 2022.

- Maintaining an appropriate scale and assessing the visual impact of that scale through adaptation to the design of upper floors
- Sympathetic materiality
- Simplified palette
- Activation of Brick street through glazing and landscaping
- Stepping of the building into the site slightly to maintain a street edge and provide space for street trees
- Concentration of areas of 'dead frontage' necessary to facilitate essential functions such as plant into the site itself rather than facing the public environment

#### 6.9.7 Influencing Travel Behaviour (ITB)

No objections subject to the proposed provision of an Electrical Vehicle Charging enabled car club space on street to be secured via the Section 106 agreement along with provision of a residential travel plan fund sum of £30662.01 and travel plan review fee of £3815.00.

#### 6.9.8 Flood Risk Management

Flood Risk Management advise no objections, the application has been accompanied by an acceptable Flood Risk and drainage assessment.

#### 6.9.9 Highways

No objections subject to conditions, provision of a s278 works scoping plan / land dedication plan, further details on refuse collection and contributions secured via the section 106 agreement relating an offsite contribution for local infrastructure improvements (£29,400) and fees associated with retrofitting future traffic Regulation Orders (£20,000)

#### 6.9.10 Contaminated Land

No objections subject to conditions concerning the submission of a phase 2 site investigation and the provision of verification reporting.

#### 6.9.11 Wind and Microclimate Peer Review (Windtech Consultants)

The application has been supported by a Computational Fluid Dynamics (CFD) based Wind and Microclimate Assessment in accordance with the Draft Wind and Microclimate Toolkit requirement for a 30m tall scale of building. No safety impacts have been identified outside the application site with only one minor exceedance of safety criteria within the garden seating area within the site envelope. Windtech have advised the exceedance is very minor and can be addressed through a condition to provide details of low-level mitigation measures within the garden area which will address the low frequency minor exceedance.

#### 6.9.12 Landscape Team

Trees around the periphery of this site are strategically important, with high public amenity value and are on the more heavily trafficked side of the application site where they undoubtedly make a contribution to mitigating the impact of the highway infrastructure in terms of air quality and carbon. The Landscape Officer has assessed the scheme and considers the proposals would likely involve the removal of 10 trees

in total within the site ( 7 are proposed for removal however taking into account the need for service ducting and construction 10 to be removed is a more realistic assessment). The inclusion of 5 replacement trees as mitigation is therefore considered insufficient in both amenity and climate change terms and regard must be had to removing mature trees and their replacement with newer specimens which take time to mature and yield carbon sequestration benefits.

Given the constrained nature of the application site, onsite mitigation for their loss cannot be delivered whilst still providing a viable footprint for re-development and therefore a financial contribution based on the CAVAT methodology is required as mitigation. This will then provide a sum for replanting / redeployment of trees in the locality. This mitigation sum must be secured through the section 106 agreement. Conditions are recommended in respect of full details of landscaping, sub ground conditions for new trees and protection details for retained trees.

#### 6.9.13 Environmental Health Services (Pollution Control)

Full details of mechanical ventilation / sound insulation and impacts and mitigation relating to potential overheating will required. It is proposed to deal with these matters by condition.

#### 6.9.14 Environmental Studies (Transport Strategy)

No objections subject to the submission and approval of glazing and ventilation details prior to occupation of the development, consistent with the site's location close to highway and railway infrastructure and the comments of Environmental Health.

#### 6.9.15 Sustainable Development Unit (Climate Change)

The proposals are considered acceptable in that evidence has been provided that demonstrates the development will meet policies EN1 and EN2 within the Core Strategy. Conditions can be used to secure full details and ensure compliance.

#### 6.9.16 West Yorkshire Police

The building and site layout has been designed to create a building which is a safe, and secure environment for residents, visitors and passers-by. It seeks to make a beneficial contribution to the prevention and fear of crime and promote enhanced security within and around the building through a range of measures. It is further recommended that access is controlled by phone QR codes; CCTV should cover all entrance and exit points and glazed areas are protected from vehicle strike. West Yorkshire Police agree with officer recommendations to closely assess the specifics on locations for lighting, CCTV and other safety provisions as part of the assessment of landscaping proposals at condition discharge stage.

#### 6.9.17 Employment and Skills

No comments received

#### 6.9.18 Access Officer

No objections, the scheme delivers accessible dwellings in accordance with Core Strategy Policy H10.

6.9.19 Ramblers Association

No comments received

6.9.20 Health Partnership

No comments received

6.9.21 Public Health

No comments received

6.9.22 West Yorkshire Ecology

No comments received

6.9.23 Local Plans

Detailed advice has been provided regarding the interpretation of adopted policy in respect of Co-Living proposals This advice is reflected in the commentary provided at 9.1 – 9.3 of this report.

6.9.24 District Heating

The District Heating team has advised this site is ideally placed for connection to the Leeds PIPES District Heating network. It is also noted that in combination with Solar PV and Air Source Heat Pumps, District Heating is identified as part of the overall energy solution for the new building. The team has been engaged with the applicant's energy consultant throughout the last year and has provided a connection proposal, which includes a commercial offer for heat sales. In accordance with policy EN4 (iv) details of reservation of space for a future connection to the District Heating Network will be controlled by condition.

6.9.25 West Yorkshire Archaeological Service (WYAAS)

WYAAS advise that the application site was developed as a hostel in the late 20th century on the site of an early 20th century tenement block, Marsh Street Garth Tenements, which was in turn built on earlier terraced streets and courtyards of workers' houses (West Yorkshire Historic Environment Record MWY7800).

Due to the lack of communal space the tenement had communal space on land to the west of the tenements, this had formerly been a graveyard. No graveyard is shown close by on historic maps and it is thought this must refer to St Peter's Church. Based on this information there are currently no significant archaeological issues associated with the proposed development.

## 7.0 **Public Response**

The application proposals were publicised in accordance with statutory requirements by Site Notice on 22.08.2022 and in the Yorkshire Evening Post on 05.08.2022. Two public letters of objection were received against the originally submitted proposals as follows:

## Objection One

- 7.1 *'The featureless wall at ground/street level is of a very poor quality design. There should be active frontages here, or at least more visual interest.'*

*View 4 and View 5 show elevations almost void of windows. This also looks poor. The large blank walls look oppressive. More windows would create visual interest and would have an improved impact on the streetscene and the surrounding area. Amendments must be sought before planning permission is granted'*

## Objection two

- 7.2 *'A very disappointing proposal for a key gateway site into the east of Leeds city centre. There are two gable end walls that are blank which face key roads and offer very little in terms of architectural merit to the onlooker, whether it be on the train entering the city from the east, or on many of the roads which run past this proposal. The design is rather dated and as said, lacks architectural merit. If the height was taller and the blank walls had windows, balconies included, with improved materials externally, then this would improve the overall look.'*

- 7.3 Based on the originally submitted drawings, Leeds Civic Trust (LCT) provided the following letter of objection (although many comments are considered to be superseded by the substantial series of revisions, the comment is provided in full to provide members with context to LCT's second letter of objection which follows):

*'The Leeds Civic Trust Planning Committee has considered the above Planning Application for proposed demolition works and erection of 11-storey building to create co-living residential development comprising of 189 apartments, and strongly objects to the proposal on the following grounds:*

*Although the applicant states that the site is adjacent to a tall building zone, it is neither adjacent as such, nor appropriate for tall buildings in the context of Brick Street and St Peter's Place on the other side of the railway arches. This is an area of heritage significance that should not be overshadowed by generic development. At best, the area is an area of transition between taller buildings to the east and more traditional lower rise buildings to the west. As such a building of half the height may be acceptable.*

*The proposal strategy should be designed to positively enhance the neighbourhood and interact with it. It is also very problematic in terms of its relationship with Leeds Minster, which should retain architectural prominence, especially in the context of aspirations shared by Leeds City Council and other stakeholders to regenerate the area through landscaping, considerable traffic and roadspace reduction measures and even the possibility of revealing Lady Beck, all as described in Leeds Civic Trust's Eastside initiative.*

*As a key building at the entrance to Leeds City Centre, the architecture has none of the characteristics that would define it as a landmark, least of all one that would represent Leeds. The diagram in page 42 of the Design and Access statement only serves to reinforce the importance that any new building in the area would have in terms of views from a number of different directions, and these need to address the architectural importance of the location.*

*As proposed, the architecture looks similar to any of the developments that have been built or proposed in the last two decades, and contributes little to the image of Leeds*

*as an innovative and pioneering city. There is no case made for enhancing the existing structure, which is also of a residential nature and should be appropriate for meaningful improvements, including the erection of two or three additional storeys and a more elaborate and distinctive outer skin. In the context of climate change and the rapid acknowledgement that retrofit and repurposing of existing buildings is inherently more sustainable in terms of carbon footprint, this should be a showcase for architectural innovation, at a fraction of the costs, not only in financial terms but more importantly in terms of impact on finite natural resources. The options for reuse of the existing structure should be properly explored as part of the application.*

*While we would, in principle, welcome a brief for co-living instead of student accommodation, the communal areas are currently very vague in terms of use, and located in such a manner that, instead of constituting a spatial heart for each floor, they are indistinguishable from private rooms and significantly distanced by long corridors in the majority of cases. The ground floor level in particular is almost exclusively dedicated to plant room and other utilitarian matters, and does not attempt to contribute to street life and place making - the building turns its back on the Minster and the city centre more generally rather than addressing it. There should be more active frontage, particularly to the South of the site, where future landscaping would be conducive to appropriation of landscape amenities during the summer months.*

*There could also be more active frontage on Brussels Street, taking into account of the architectural merits of the existing railway arches, are already partially occupied with some active uses and may become more active in future. In conclusion therefore, the proposal is not reflective of its intentions as a gateway and landmark building, and should be re-designed to take into account all the comments listed above.'*

7.3 In addition to the above, a second letter of objection has been received from Leeds Civic Trust (LCT) as follows:

*'The Leeds Civic Trust has reconsidered the above application for proposed demolition works and erection of 11-storey building to create co-living residential development comprising of 189 apartments, which has been considerably amended. Despite a new massing that is arguably slightly less generic than the previous iteration, our original grounds for objection still stand, particularly with regard to the treatment of the largely mute ground floor without meaningful relationship to the ground. We note that the new visualisations exclude a close up, which serves to acknowledge that the developer is aware of this fundamental absence of place making on a key gateway area. We also note that no views have been provided that take into account the context of the conservation area on the other side of the railway arches, nor that of the Minster. In both cases, this would highlight the fact that the massing is still disproportionate to its neighbours.'*

## 8.0 KEY ISSUES

- Principle of development / Co-Living
- Housing Mix and Density
- Affordable Housing
- Design and Heritage considerations
- Residential Amenity
- Accessibility and Inclusivity
- Landscaping and Public Realm proposals / Green space
- Transportation Considerations
- Sustainability and Climate Change
- Biodiversity
- Wind and Microclimate Considerations
- Safety and Security / Management
- Planning Obligations and CIL
- Representations
- Planning Balance and Conclusion

## 9.0 APPRAISAL

### 9.1 Principle of development / Co-Living

- 9.1.1 The site is located within the designated City Centre and is unallocated within the Site Allocations Plan. CS Policy CC1(b) encourages residential forms of development in City Centre locations providing that the development does not prejudice the functions of the City Centre and that it provides a reasonable level of amenity for occupiers. The proposals are considered meet these criteria subject to the detailed planning assessments which follow. Leeds City Council has now also adopted a Co-Living Advice note which recognises the need to consider co-living proposals subject to detailed amenity and infrastructure considerations.
- 9.1.2 Policy H2 of the CS states that windfall sites will be acceptable in principle providing the number of dwellings does not exceed the capacity of transport, educational and health infrastructure, as existing or provided as a condition of development. The site is sustainably located in terms of transportation and local amenities and would involve the re-use of a brownfield site, which is positive in respect of Spatial Policy 1 (requiring the largest amount of development to be located in the Main Urban Area and prioritising previously developed land and buildings). This is consistent with the requirements of paragraphs 123 and 124(c) of the National Planning Policy Framework.
- 9.1.3 Health Partnerships have been consulted on the application proposals however no formal response to the consultations have been received. Officers are however aware that Health Partnerships have (as part of the consultation process for recent applications at Quarry Hill) identified local GP practices and have very recently advised that the two closest surgeries (York Street and The Light) both currently have capacity, both are in close travelling distance to the application site and are accepting new patients.
- 9.1.4 Healthcare Partnerships have recently drawn officers' attention to the strategic need to future proof healthcare provision and the need for a plan to be put in place to respond to growth. The Health Partnership Team have advised that NHS GP funding operates as a "post hoc" mechanism in the NHS and there is currently no NHS mechanism that local NHS officers have at hand to "front load" capital infrastructure.

However, it was also advised that the impact of 'e consultation, "telehealth' and enhanced pharmacy functions mitigate some of the square footage requirements in the traditional doctors surgery model. ICB (NHS West Yorkshire Integrated Care Board) have also recently noted estate constraints emerging in local health infrastructure and it has been queried whether there is scope to secure capital support through more recent planning applications to support extensions in clinical space, specifically highlighting LSMP seeking to support and improve their provision through investment where possible. This request was also made in the response to the recent application 23/02335/FU for PBSA accommodation at 16-22 Burley Street, determined by members at City Plans Panel on 2<sup>nd</sup> November 2023.

- 9.1.5 A legal test for the imposition of planning obligations was introduced by the Community Infrastructure Levy Regulations 2010 regulation 122 (as amended in 2011 and 2019). This provides that a planning obligation may only constitute a reason for granting planning permission for the development if the obligation is:
- a. necessary to make the development acceptable in planning terms,
  - b. directly related to the development; and
  - c. fairly and reasonably related in scale and kind to the development.
- 9.1.6 Notwithstanding that there may be benefits for individual patients associated with expanding GP surgery estate to enable a wider range of primary care to be accommodated within a particular GP practice / service, no specific details have been provided of what is needed in the case of those practices wishing to expand their estate capacity and the likely costs involved. It is also important to note that the above identified local GP practices are known to be taking on new patients. In this context it is considered that in terms of Healthcare, the proposals are acceptable.
- 9.1.7 In terms of educational infrastructure, by its nature this Co-Living proposal would not attract family living. Unit sizes are too small to accommodate families and are targeted at the single occupier. Therefore, it is considered that the proposals meet the requirements of adopted policies in terms of educational infrastructure.
- 9.1.8 In the absence of existing formally adopted planning policy on co-living development Development Plans Panel agreed to the preparation and adoption of the technical advice note to aid consideration of co-living proposals in a consistent manner (reflecting existing adopted policies on residential use). The proposals are considered to accord with the guidance note as follows.
- 9.1.9 In the case of this specific development at Holdforth Court, the applicant has elected to provide individual unit sizes at the upper end of the range of scales seen in other national Co-Living Schemes. The units considered here in fact provide studio accommodation that would meet the adopted space standards for single person occupancy. Each unit in this scheme would contain all of the expected provisions for day to day living, with the operator intending to promote the aforementioned sense of community and deflect isolation of residents through their building and tenant management processes and provision of shared communal spaces.
- 9.1.10 In this case, the kitchen spaces would supplement and compliment generous individual scaled studios and there would be less reliance on the communal spaces to achieve a baseline and satisfactory level of amenity for the occupier compared to a scheme with small studio sizes. The proposals would also meet affordable housing requirements as set out in the guidance note as well as provide a travel planning regime and accessible units.



9.1.11 Access and provision of shared day to day living facilities/functions are evident and would be easily accessible to residents in the overall accommodation. Based on the submitted layouts, tenants would have easy and local access to shared facilities by floor and all private studios would be of sufficient size for sleeping, bathroom functions, general manoeuvring space and private storage. Remaining amenity considerations along with management considerations are set out below, however it is considered that in this regard, the use of the larger studio format meets the requirements of the advice note and provides an acceptable overall principle of development subject to the detailed controls set out in the conditions and remainder of this appraisal.

## **9.2 Housing Mix and Density**

9.2.1 A minimum density policy is needed for Leeds to ensure sustainable housing development. This means efficient use of land in order to avoid more greenfield land being developed than is necessary and in order to achieve a higher population in proximity to centres. The proposals meet the requirements of adopted policy H3 and it's threshold of delivery of at least 65 dwellings per hectare.

9.2.2 The proposed development is 100% Flats. Paragraph 5.2.11 of the Core Strategy states that the:

*'...The form of development and character of area should be taken into account too. For example, a scheme of 100% flats may be appropriate in a particular urban context...'*

In this case it is felt that a 100% flat scheme is appropriate given the nature of the City Centre and the compact site to which the development relates.

9.2.3 As noted above due to the nature of the co-living proposal before members, the proposed housing mix differs from traditional residential schemes through the provision of 1-bed studio apartments throughout the scheme, creating private space for each occupant who would then access shared communal space on each floor as part of the co-living model. The proposal therefore unambiguously fails with regard to the targets set out in policy H4 of the Core Strategy with regard to two, three and four bed properties. Officers have not sought the revision of the scheme on the basis of this shortfall against the policy as multiple bedroomed units are simply not compatible with the residential co-living model. It should also be noted that the number of units fails to trigger the policy and adopted development plan's requirement for the submission of a Housing Needs Assessment at the scale and quantum of units proposed.

9.2.4 The Local Planning Authority does not yet have its own comprehensive dataset for assessing the need for co-living accommodation in the city. 2021 census data for the breakdown on house share data is not yet published, however given the known percentages weighted towards single and 2 person households (20 – 44 age group which in Leeds accounts for 37% of the city's population, one of the highest groups of working-age renters and the number of recorded private rented households is around 74,400) it can be reasonably concluded there is potentially a market (however limited) for this type of accommodation in Leeds. This perceived need has further manifested and evidenced with other co-Living proposals (albeit as variants to the specifics of the model here) being proposed to the Local Planning Authority at Headrow House and Burley Library in recent months.

- 9.2.5 The applicant has put forward data acquired and publicised by Savills as justification who estimate there are approximately 32,000 people within the core target market for Co-living in the city at the present time and given the demographic within that market (surveyed by the applicant as approximately 50% of the market at 25-34 years of age and approximately 25% being 18-24 years old) the mixed use nature and vibrancy of the City Centre, it's amenities and access to transportation and places of work make it an obvious location for this accommodation type. Also, the product is targeted at the recent graduate market, consultancy-type workers who only need to be in a particular location for a few months, key workers, and also recent incomers to cities who don't necessarily want to rent on their own or know anyone to house share with. The occupancy isn't restricted however to particular groups like graduates or key workers. Again the city centre and it's periphery lends itself to addressing this need in terms of the character of the area and its facilities.
- 9.2.6 The Co-Living advice note makes clear that co-living proposals will not deliver a mix of unit sizes and accepts that co-living accommodation by it's nature is aimed at non family accommodation. Officers therefore consider that on its own merits, this scheme (although by default being non-compliant with policy H4) is acceptable on it's own individual case by case merit based upon the advice note and that the clear non-compliance against housing mix policy is in this case outweighed by the other identified planning benefits of the scheme which will be set out in the remainder of the report.

### **9.3 Affordable Housing**

- 9.3.1 As noted in the Co-Living Technical Guidance note, the council have a clear expectation for major development (10 or more dwellings, or where the number of dwellings is not known, a site area of 0.5ha or more) to contribute towards affordable housing. In many cases, given the model's departure from more standard residential layouts and its dependency on shared facilities, it may be determined that the most appropriate approach for Co-living schemes to contribute is a commuted sum in lieu of onsite provision, which is also a common approach with Build to Rent developments within the City Centre and is supported by the advice note as an acceptable approach.
- 9.3.2 Policy H5 is identified as a relevant policy and on which the advice note's approach to affordable housing is based. Arm (iii) of the established policy allows for Build to rent type developments (which is the nearest approximation to Co-Living) to address the affordable housing requirement through a commuted sum where suitable justification against on-site delivery can be provided. In this case, the co-living nature of the scheme and the operators the applicant is engaged with have no experience in managing affordable dwellings within an entire scheme and therefore the applicant would like to select to address the affordable housing provision by way of contribution to allow for the units to be provided elsewhere in the city as need determines. The guidance note allows the approach to affordable housing to mirror that established in policy H5 for Build To rent schemes which share similarities in nature to co-living products.
- 9.3.3 From an operational view the provision of separate small number of affordable units spread across a larger development is known to be very difficult to manage even for experienced affordable housing providers within the context of the overall management of the whole development. The applicant has advised that Co-Living operators they are engaged with do not have the relevant experience/training with their staff resources and would look only to operate and manage the development

excluding the affordable units. Therefore; an approach consistent with policy H5 arm (iii) and in accordance with the advice note, the use of a commuted sum allows the applicant to be policy compliant in this regard.

- 9.3.4 The use of the benchmark rate for affordable rents for Build to Rent (BTR) developments is suggested within the Co-Living Guidance note as an appropriate approach for off-site contributions, given that the management, operation and anticipated occupancy of co-living developments are very similar to that of BTR. This is based on local earning levels and applied as a price per sqm. The applicant has proposed a commuted sum figure of £742,633. At the time of this report, this proposed sum and the methodology of it's calculation is in the process of validation through the District Valuer's office (as is standard procedure) and may be subject to amendment or variation through that process in due course.

## **9.4 Design and Heritage considerations**

### Townscape

- 9.4.1 As has been addressed in earlier discussions with the agent concerning this site, in terms of the wider context, the application site is identified as not being appropriate for a 'tall' building akin to those built, permitted or proposed to the eastern side of Marsh Lane and south of the site along the southern edge of Crown Point Road (Ibis/Gateway). The building is not within an identified area for tall buildings within the current adopted version of the adopted Tall Buildings SPD and therefore proposals for taller buildings are addressed through a contextual analysis of the surrounding area. The key townscape considerations in relation to the proposed building are considered to be as follows:
- 9.4.2 It is considered that based upon the topography of the site and surrounding area and in accordance with Quarry Hill's categorisation as a 'Gateway Location' in the Tall Buildings Design Guide, the dominant point of height in north-south and east-west views should be Quarry Hill. The proposed and revised 10 storey height and massing preserves this view in the key vista looking northward from Crown Point Bridge). Whilst the SOYO development and other planned developments will (and are) increasing the overall apex of Quarry Hill in the wider townscape it is important that this site and sites immediately adjacent remain visually subordinate to the context of Quarry Hill. The proposed materiality of building (predominantly brick and glazing) is considered appropriate given the diverse tapestry of materials in the emerging and long-established built context around the site to the east and to provide some visual anchorage to Quarry House which sits in the backdrop of views northward from the south.
- 9.4.3 The area between the application site and Leeds Minster/High Court is considered to be an area of relief from buildings and built form, with a lower scale buildings (2-4 storeys) along the southern edge of Kirkgate representing a step down from south to north from taller buildings to the southern edge of The Calls towards this area of 'respite and calm' in the setting of the Minster. The building is set out at 10 floors in total, with the ground floor providing the developments ancillary functions, such as cycle stores and residents facilities, and the next 9 floors providing dedicated and well considered residential accommodation, with a large 70/40sq.m resident's lounge/kitchen on each floor. The proposed building is clearly separated from the green parkland area and pedestrian routes around the minster by wide extents of highway infrastructure, but also shows a definitive drop in height from the taller context to the east towards the respite area around the minster.

9.4.4 It is considered the application site should perform a similar spatial function in grading down the height of buildings from the established and emerging taller environment to the east beyond Marsh Lane and this lower area in the setting of the Minster and should also offer some symmetry with Munro House's lower scale at the southern edge of Quarry Hill, beyond which, to the north buildings then step up in height to the identified point of height of Quarry Hill/SOYO. In response to the local surroundings and the existing built forms, the development's mass has been designed to provide 10 storeys addressing Marsh Lane and Brick Street, substantially lower than the approved neighbouring schemes at Saxton Lane which sits at 19 and 21 storeys – providing a clear step down in height between south to north and east to west towards the viaduct whilst broadly mirroring the massing heights of Northern Ballet and the approved extension to Munro House in views along Marsh Lane / New York Street. Although taller than the existing forms of development immediately the southern edge of the viaduct it is considered that the proposed height would be harmonious with the existing forms of development, serve to provide a visual transition in height between the taller character of adjacent new buildings and the southern edge of Quarry Hill and is acceptable in townscape terms.

#### 9.4.5 Heritage Considerations

Section 66 of the Planning (Listed Buildings and Conservation Areas) Act 1990 states that decision makers should give considerable importance and weight to the desirability of preserving the setting of listed buildings.

The application site is approximately 185 metres due east of the Leeds Minster and whilst of a lower scale than its adjacent peers would be recognisable as part of the prevailing modern character incorporating the Ibis (155 metres separation to the Minster) and residential buildings to East Street (127 metres) as well as the Gateway Residential Complex (177 metres) as noted above, the road infrastructure (in particular Crown Point Road and Duke Street) provide at least 4 lanes of roadway as separation distance to the landscaped public route forming an important green buffer to the environment around the Minster. It is not considered that with this level of separation and the site's placement amongst a much more recognisably modern context, the proposals would not affect the setting of Leeds Minster or the Conservation Area.

#### 9.4.6 Building Design

The ground floor has a floor to ceiling height of between 2.45m and 3.45m with a split-level ground floor which follows the topography of Brick Street. The concentration of shared ground floor spaces to the eastern edge of the ground floor is a deliberate design device intended to increase levels of natural surveillance to what is a visually dead area / frontage at present - with the intention of activating Brick Street and introducing greater surveillance onto a route which is likely to become more intensively used given its proximity to the emerging SOYO residential led development at Quarry Hill and a key route between that area, the riverside and onward connections to Crown Point, Meadow Lane and the South Bank.

The feature band at first floor level visual breaks the building at circa 4.3m, with a second break defining the "middle" brick element of the scheme at circa 23m. The building then steps in with fully glazed top floors completing the development and a roof level approximately 30m above ground. The change in materials and fenestration pattern clearly define the 'top, middle and bottom' approach to the scheme and the glazed format of the roof levels will assist in lowering the overall dominance of the building in respect of the wider townscape.

With the help of precedent-setting materials and architectural details from the emerging adjacent neighbourhood, the building has been designed to produce a high-quality, contemporary addition that blends in with the surroundings. Modern features include angled reveals, decorative brick detailing, copper-coloured metal façade panels, feature slot windows and glazed top floors.

The primary elevations are proposed to include angled reveals and slot windows that emphasise vertical elements, resulting in a rhythmic fenestration pattern. The building's crown of reflective glass completes the last two stories, which are set back from the main elevations. This change from a solid (ground) to a lighter top helps to diminish the building's perceived mass. The building has been considered in respect of the approved schemes at Saxton Lane, where the emphasis on elevational design is placed heavily on vertical slit windows. Conditions will be employed to finely control the quality of facing materials, depths of window reveals; and construction standard drawings will be required at condition discharge stage to specifically analyse a number of junctions and proportions of the buildings tertiary architectural features.

## **9.5 Residential Amenity**

- 9.5.1 Notwithstanding the Co-Living format proposed, the assessment of amenity is a wider consideration of qualitative factors including arrangement and separation of living functions (general living, sleeping, studying, eating, cooking, food preparation, storage and circulation), usable shape, outlook, privacy and external amenity space.
- 9.5.2 In spatial terms it is considered in this case that the individual residential units would provide for acceptable levels of internal space and a good standard of residential amenity irrespective of consideration of the buildings additional shared facilities. The submitted plans clearly demonstrate each unit has capacity for hosting all expected day to day facilities required for self-contained accommodation listed above and provide for acceptable levels of outlook and light receipt. Units commonly feature two external facing windows per unit and concentrate the principle living areas for cooking and cleaning where natural light would be more desirable towards the exterior face of the building and bedroom / toilet spaces to the interior core. Views out of windows commonly involve vistas of the adjacent street and highway infrastructure and do not presently involve any proximate buildings which may adversely impact upon the privacy of the occupier. The expectation is that the future development of any surroundings for residential or other uses would have to have regard to the placement of windows within this proposed residential building as adjacent redevelopment would be the agent of change and would need to be designed to accommodate and preserve the residential amenity of the occupiers of this building should proposals come forward in the surroundings in future years.
- 9.5.3 Supplementing these private spaces are shared lounge / kitchen facilities at each floor equating to 70 sqm per floor. Each kitchen space is shared with 14 units per floor and will allow for the congregation, cooking, collective endeavors and activities commensurate with the Co-Living model and contributing to a framework for communities to organically form amongst residents. It is considered that the further additional facilities at ground floor would further reinforce and promote that sense of community amongst residents and the applicant has had clear regard to the provision of a variety of functions rather than an anonymous extent of unspecific shared space.
- 9.5.4 Following a substantial revision in regard to building design and layouts received in September 2023, an updated noise report has been issued. The updated noise report details baseline measurements of environmental noise and recommendations on glazing and mechanical ventilation to provide acceptable internal sound levels – of

importance due to the proximity of heavy road infrastructure and the adjacent environment of the railway.

- 9.5.5 The noise report demonstrates that closed windows will be required along with the provision of MVHR to meet Approved Document F for background ventilation. Further information is required that quantifies overheating risk via dynamic thermal modelling along with a narrative on how overheating will be further minimised in units that are at risk whilst maintaining acceptable internal noise levels. This information will be controlled through conditions.

## **9.6 Accessibility and Inclusivity**

- 9.6.1 New build schemes should incorporate an inclusive design approach which recognises that people are all different and accommodate this diversity by placing people at the heart of the design process from the outset, creating solutions that provide choice as to how people access and use buildings or spaces, and enable everyone to use the development safely, easily and with dignity. This approach is in accordance with the Equality Act 2010 and the associated Public Sector Equality Duty.
- 9.6.2 Policy H10 requires that 30% of residential dwellings within Building C meet M4(2) standards and 2% of dwellings meet the higher M4(3) 'wheelchair' user standard of part M of the Building Regulations. The proposals meet this policy requirement in full.

## **9.7 Landscaping and Public Realm proposals / Green space**

- 9.7.1 The site is located in the city centre and is covered by Core Strategy Policy G5 which seeks open space provision on sites over 0.5ha. The site is 0.13ha so falls below the requirement, but provision of any external green space is still encouraged as an essential amenity for the proposed residents. Holdforth Court is located on a tight urban plot with limited scope to provide meaningful onsite public and private amenity space, landscaping or biodiversity improvements, but this matter has to be clearly weighed against the status of the site as Brownfield Land in a sustainable location and existing urban context which local and national planning policy and guidance suggests should be prioritised for development in sustainability terms.
- 9.7.2 There is a significant belt of Green Infrastructure including trees and other vegetation to the South West. These are strategically important, with high public amenity value and are on the currently more heavily trafficked side of the application site - where they undoubtedly make a contribution to mitigating the impact of the highway infrastructure in terms of air quality and carbon sequestration. These trees are outside of the application site boundary and are to be retained along with two mature trees at the site's northeastern corner which have high amenity value and will in conjunction with newly proposed street trees along Brick Street provide for an improved public environment within an area likely to see increased public footfall in future years given the emerging context of high density residential development in the locality, including at Quarry Hill.
- 9.7.3 It is noted that in order to facilitate both the increased footprint of the proposed building and create a meaningful amenity space for residents which receives good levels of sunlight, a series of trees within the site boundary are programmed for removal. In this case, the site cannot accommodate their relocation given its compact nature and the proposed level of street trees the adjacent environment is capable of hosting does not equate to the 3 for 1 ratio of replacement commonly used to mitigate, nor would

the environment of brick street and it's limited overall width and the need to maintain a viable pedestrian footway width allow for the use of non-columnar trees which provide the best amenity and biodiversity/climate change benefits. Notwithstanding, new street trees are proposed as some benefit is considered to be better than none and despite a lack of significant benefit in terms of carbon sequestration, new trees will provide improved amenity value to Brick Street and provide a better setting at the human scale than the existing arrangement (in accordance with NPPF paragraphs 96 and 136 which place emphasis on active frontages and the provision of tree lined streets where practicable).

In this case, it is considered that the proposed level and nature of loss cannot be accepted without further mitigation beyond the replacements / new trees proposed. The Landscape Team have provided a CAVAT valuation of the affected trees which equates to £77,809.00 to be secured through the Section 106 agreement. These funds can be used to provide additional planting offsite to offset the tree loss in both amenity and climate change terms and make the development acceptable in planning terms. Whilst tree retention is always the clear preference, in this case, it is considered that a viable activation of the site for residential use without significant tree loss would not be achievable and that this mechanism is proportionate and can deliver wider planning benefits which outweigh the loss.

- 9.7.4 The site is under half a hectare in size and located within the designated city centre boundary. Therefore, in accordance with Core Strategy Policy G5 open space provision is not sought.

## **9.9 Transportation Considerations**

- 9.9.1 Through conditions and the associated legal agreement the scheme is considered to constitute an improvement to the present pedestrian and vehicular environment. 3m footways are provided along the frontages of the building which is in accordance with the adopted Transport SPD and the accommodation of new street trees and active frontage will allow for improved natural surveillance.
- 9.9.2 No vehicular parking (beyond provision of a disabled parking space) is proposed which raises no concerns in principle considering this site meets the accessibility standards set out within the Core Strategy and is located within the city centre core area. Streets immediately around the site contain TROs, however there may be opportunities for residents to park on-street further away. In order to prevent unforeseen parking problems the developer is required to carry out a survey on roads within 800m of the site showing existing on-street parking levels. If the council believe a parking problem has occurred due to vehicles associated with the site a 2nd survey may be required. A fee of £20,000 will be secured through the Section 106 agreement to mitigate against future parking issues caused by the development and to assist in amending existing Traffic Regulation Orders to accommodate a new Car Club bay detailed below.
- 9.9.3 A Travel Plan has been provided which details a series of incentives and the provision of information to residents concerning transport modes which are alternative in nature to the private car, encouraging the use of public transport, cycling and walking and taking advantage of the sites highly sustainable location in terms of bus and train use. A car club space on street in the locality is also to be provided through this consent. Officers are presently working with the developer and the Influencing Travel Behaviour Team to identify a suitable set down location for an Electrical vehicle enabled space for this provision. It is expected that the Car Club bay will be provided

on the eastern side of Brick Street behind the existing loading bay and feasibility work on this matter is ongoing. This will be secured through the S106 agreement.

- 9.9.4 The application is supported by a Transport Statement and drawings which advise the provision of 108 cycle parking spaces. This figure falls slightly short of according with the requirement of the Transport SPD which suggests 118 spaces as an acceptable figure. Of the cycle parking spaces provided 14 are shown to be Sheffield stands which equates to around 11% however a minimum of 30% of the overall provision must be in the form of Sheffield stands and this matter along with the finer detail of facilities will be controlled through conditions. As always, higher quality cycle parking will be preferable to quantity therefore the number of Sheffield stands should be increased and this will be insisted upon through the condition mechanism. At the time of this report, a proposal has been suggested by the applicant for a series of rental bike spaces to be also made available as part of the overall provision. This will be given consideration prior to any formal approval, appropriate controls applied and the required level of detail attained.

The developer has agreed to contribute towards the improvement works at the Kirkgate / High Court junction and along High Court as this will improve the environment for residents of the site. The scheme will extend the cycle lane on Kirkgate and provide a new zebra crossing across Kirkgate which will improve safety of residents heading into Leeds city centre from the application site. Footways around the junction are also to be widened which will improve the environment for pedestrians.

The scheme will cost £1,500,000, the developer is expected to contribute 1.96% of the overall cost of the scheme, which equates to £29,400. This contribution will be secured through the section 106 agreement and will assist in mitigating the impacts of the development on the pedestrian network locally through the increased footfall the development will bring to the area.

## **9.10 Sustainability and Climate Change**

- 9.10.1 Members will be aware that the Council has declared a Climate Emergency. Existing planning policies seek to address the issue of climate change by ensuring that development proposals incorporate measures to reduce the impact of non-renewable resources.
- 9.10.2 The proposals are stated to meet the requirements of planning policies EN1 and EN2 to reduce total predicted carbon dioxide emissions (to achieve 20% less than the Building Regulations Target Emission Rate and provide a minimum of 10% of the predicted energy needs of the development from low carbon energy). The applicant is proposing to utilise a connection to the District Heating Network in order to meet the requirements of EN1(ii) and EN4 – conditions will be used to control the details of the connection or details of a viable alternative to the connection through other measures should the connection not be possible, albeit engagement between the District Heating Team and the applicant would suggest that a connection in this location can be provided. Details of the location and timescales proposed for a connection must be submitted prior to commencement of construction on site and will be controlled by condition. Full details of water calculations to ensure compliance with EN2 will be achieved will also be controlled through conditions. The applicant has advised that as well as a planning policy requirement the specifications required to achieve the 110 litre per person per day are also controlled under the building regulations to that effect and therefore has every intention to deliver this.



As required by the NPPF, proposed new developments should contribute to, protect, and enhance the natural and built environment. A holistic look at the whole life cycle carbon emissions favors re-use and refurbishment of existing building stock. Not retaining whole/ parts of the existing building would obviously add to the carbon emissions of the scheme. The following rationale is considered to justify the demolition of the building.

- The existing layout is based on small rooms (former hostel use). Many of the rooms are subdivided with structural walls and are therefore not suitable for conversion or would require significant built intervention at a substantive cost.
- The existing layouts include lots of areas of dead space, based on previous use and would be difficult to bring into use.
- The existing building is c.30 yrs old and would require a substantive thermal upgrade, again affecting layouts/unit numbers and therefore the viability of a residential scheme.
- Existing massing , number floors would not allow sufficient units to make it viable to re-use. The new proposal involves more floors and reaching the critical mass needed to make a redevelopment of a brownfield city centre site such as this possible.
- The existing fenestration and window apertures are poor where measured against current standards and the expectations for residential accommodation to deliver a high standard of residential amenity in accordance with policy.
- Plant space requirements would take up much of the existing ground floor plate, again affecting the number of units that can be realistically achieved.
- Utility upgrades will be required in order to suit modern needs – the existing configuration is not adequate. A new substation and a connection to the district heat network would take up too much space affecting units/layouts.
- The Existing floor to floor levels not conducive to incorporating the latest and most efficient M&E standards/requirements, such as sprinklers/District Heating network connection.
- Fenestration /openings/facade not conducive to addressing noise and MVHR requirements which is of particular importance given the building's location close to heavy transport infrastructure.
- In allowing demolition of the building, a recycled waste strategy will be controlled by conditions in accordance with policy EN6.

## **9.11 Biodiversity**

The site is subject to assessment in accordance with the NPPF and adopted local policy Core Strategy Policy G9, whereby the development needs to demonstrate a net gain in terms of Biodiversity. However, given that the application has been under consideration for some time, the site is not required to demonstrate the 10% uplift required by new national policy which comes into force on 12<sup>th</sup> February 2024 as required by section 90A of the Town and Country Planning Act 1990 as amended by

the Environment Act 2021. At the time of this report, metric calculations are being undertaken by the applicant who has committed to provide a positive uplift in terms of net gain and if this cannot be provided on site, has committed to the purchase of an offsite credit in accordance with the new national policy. In the event this was required, this would be secured through the section 106 agreement.

## **9.12 Wind and Microclimate Considerations**

9.12.1 Due to the scale of the proposed buildings and their largely exposed location there is a significant potential for the generation of strong winds around the development. The applicant engaged wind consultants at the commencement of the design process to provide advice regarding the safety and comfort of the wind environment resulting from the development. As summarised in the consultation responses above a CFD assessment was made as part of the process (in line with the requirements of the Draft Wind and Microclimate Toolkit) and the findings were peer reviewed on behalf of Leeds City Council by Windtech Consultants. Only low-level mitigation within the application site itself will be required to address a very minor exceedance against safety standards affecting and limited to the garden area of the development site. This matter will be controlled by condition. In all other respects the application proposal is not considered to introduce wider impacts in terms of safety.

## **9.13 Safety and Security / Management**

9.13.1 CS policy P10(v) identifies that developments should create safe and secure environments that reduce the opportunities for crime and the NPPF states that developments should be safe and accessible so that crime and disorder, and the fear of crime, do not undermine quality of life. On this basis, two phases of consultation have taken place with Yorkshire Police's Architectural Liaison Officer.

9.13.2 The proposals identify a predominantly car-free development that would result in a significant number of people walking and cycling to the site. Off-site public realm works to improve the pedestrian and cycling corridor north-west of the development will bring additional movement and activity. These improvements will link with similar proposals on neighbouring sites which will result in a significantly improved environment and which should reduce the fear of crime as noted by West Yorkshire Police within their initial consultation / discussions, whilst encouraging use by pedestrians and cyclists, creating a hospitable environment and promote natural surveillance.

9.13.3 West Yorkshire Police have made further recommendations concerning the need for either the use of bollards or fixed planters to the Brick Street / Brussels Street elevations of the site given the prevalence of glazing to the ground floor areas. This is noted, however it is considered features should be integrated with the proposed new footway and landscaping treatment / street trees where possible. The specific details will be controlled by condition and can be assessed by West Yorkshire Police, the Landscape Team and highways officers through that process to identify optimum locations, species details in the event planters are to be employed and to ensure pedestrian movement through the widened pedestrian route is not compromised and the benefits of the new pedestrian environment the scheme delivers is not diluted by their inclusion.

9.13.4 Based on comments received in both phases of consultation, CCTV surveillance will be required and will be provided around and within the site. This matter will be controlled by condition to allow for further direct consultation with West Yorkshire Police - to ensure optimum locations for lighting and CCTV are employed and to

ensure CCTV equipment provides suitable legibility and recording of images for reporting purposes. It is expected that the quality of CCTV recording equipment must accord with the British Standard as should all specifications for lighting, doors, windows, encrypted key fob access for cycle storage and building access. It is considered these matters can be secured through conditions.

- 9.13.6 Subject to detailed design to be secured by a security strategy condition and details of all built security measures the development would accord with CS policy P10.
- 9.13.7 The Co-Living advice note advises that conditions should be sought to ensure that the health and wellbeing of residents is properly considered and that regard is given to the high frequency of residents that may move in and out. Clearly the adoption of larger unit sizes within the scheme and proximate communal spaces close to private spaces will provide facilities for residents to in effect co-habit and mingle. However it is considered that due regard be also given to the potential for friction and dissonance between residents which are potential natural corollaries of communal living in any form of shared accommodation. Given that resident safety is to be addressed in terms of the building and it's wider environs as detailed above following advice from West Yorkshire Police it is considered the same regard must be had for securing appropriate procedures for resident safety within the building, how issues of resident behaviour are to be addressed and to ensure a single management regime remains in operation for the lifetime of the development to maintain comprehensive oversight of resident welfare. A management plan will be controlled by condition to address the operation of on site management, access control procedures, details of refuse collection and on site procedures for addressing anti-social behaviour.

#### 9.14 **Planning Obligations and CIL**

- 9.14.1 A legal test for the imposition of planning obligations was introduced by the Community Infrastructure Levy Regulations 2010 (as amended in 2019). These provide that a planning obligation may only constitute a reason for granting planning permission for the development if the obligation is:
- (a) necessary to make the development acceptable in planning terms,
  - (b) directly related to the development; and
  - (c) fairly and reasonably related in scale and kind to the development.

The proposed scheme produces the need for the following obligations which it is considered meet the legal tests:

- Leeds City Council Travel Plan Review fee of £ 3815.00
  - Provision of Leeds City Council Car Club provider parking space
  - Provision of a Residential Travel Plan Fund of £30,662.01
  - Offsite affordable housing commuted sum of £ 742,633(This sum will be subject to independent valuer verification)
  - Compensation sum for loss of trees and offsite replanting £77,809.01
  - Contribution towards High Court / The Calls Improvement Scheme (£29,400)
  - Provision for TRO amendments (£20,000)
  - Section 106 management fee
- 9.14.2 This development is liable to the Community Infrastructure Levy (CIL) and is likely to generate a CIL charge of £10014.12 This figure is presented for information only and should not influence consideration of the application. The infrastructure requirements for this development are likely to relate to public transport and public space provision.

Consideration of where any Strategic Fund CIL money is spent rests with the Council's Executive Board and will be decided with reference to the Regulation 123 List (or Infrastructure Funding Statement as the case may be) at the time that decision is made.

## **10.0 REPRESENTATIONS**

10.1 As noted at paragraph 7.0 of this report, two public letters of objection have been received. These objections relate principally to the originally submitted design and layout, which has now been substantially revised through negotiation and a comprehensive redesign of the scheme over the past 18 months.

10.2 Leeds Civic Trust have provided two letters of objection. Again, many issues raised in their first letter are considered to be superseded by the substantial redesign of the building. However, LCT maintain the objection in principle and a combination of retained issues from their first objection and the content of their second objection are summarised and addressed as follows:

- a) Largely mute ground floor treatment - the ground floor level in particular is almost exclusively dedicated to plant room and other utilitarian matters, and does not attempt to contribute to street life and place making
- b) Lack of provision of wider CGI views which address the impact upon Leeds Minster or the Conservation Area
- c) Massing is disproportionate to neighbours

10.3 In response

- a) In effect the building has three exterior public facing elevations and as much of the active uses such as lounge areas and workspaces have been directed to the external public facing of the building (where surveillance and a sense of visual activity have wider benefits to the overall streetscape and creation of a sense of place). It is acknowledged that cycle storage and bin storage by their nature create dead frontages, however in this case, this equates to less than 1/3 of the exterior frontage of the building with the vast majority of non-active frontage facing into the site itself. Behind these areas of frontage are necessary facilities for residents and facilities to ensure a residential use can be properly supported in transportation and amenity terms. It is therefore considered that whilst not a perfect arrangement which delivers 100% active frontage, given the shape of the site envelope and need to deliver supporting functions, the proposed arrangement is acceptable and makes best use of the street scape and floor plate of the building in this regard.
- b) It is considered that the application site's clear separation from the conservation area by virtue of partition by the large-scale highway infrastructure and the site's placement within a more modern setting of tall buildings as a backdrop to the East ensure the building and its immediate environment are clear and distinct from the conservation area and do not adversely affect it's setting, nor that of Leeds Minster at approximately 170 metres separation from the application site.
- c) It is acknowledged that the new building is taller than its predecessor and immediately adjacent neighbour however for reasons set out at section 9.4 of this report the proposals are acceptable in townscape terms.

## 11.0 PLANNING BALANCE AND CONCLUSION

- 11.1 In summary, while it is clear that there is currently no adopted policy provision, either within the Leeds Development Plan or the National Planning Policy Framework specific to co-living development; it is also clear that this is an emerging model for housing development in Leeds and other Core Cities and it is likely other similar schemes may come forward for consideration in this near future. Leeds City Council has recently adopted an advice note to assist in addressing Co-living proposals and in the absence of a specific planning policy for this emerging form of accommodation it is a material consideration. The proposals are considered to meet the guidance. In assessing this individual case on its own merits against the existing planning policy context and available guidance, it is considered that the benefits of the development outweigh any potential conflict with planning policy as the adopted advice note does not seek compliance with Core Strategy Policy H4 concerning Housing Mix. The proposals would involve a predominantly car free residential development and a scheme which allows for prioritisation of sustainable transport modes within a mixed-use city centre location (where in placemaking terms improvements to the adjacent street scene can be secured by association).
- 11.2 Tree loss whilst regrettable would activate the site for redevelopment and can be appropriately compensated and mitigated - allowing for redirection of planting to more public areas and therefore a provision of wider amenity benefits. The scheme would contribute towards much needed affordable housing in the city and would deliver a high standard of amenity for the occupier - whilst redeveloping an urban brownfield site with a building that will accord with and can be controlled to meet adopted policies on sustainability and climate change.
- 11.3 The scheme would provide further choice within the expanding portfolio of residential types and tenures in the city centre setting but would not compromise on the quality of accommodation in doing so when assessed against adopted standards for more conventional residential tenures. Furthermore; the proposals have been assessed against the requirements of adopted heritage policies and legislation and have found to be without detriment to the city's important and designated heritage assets by virtue of the clear separation of the site from Leeds Minster and the character of the site's immediate locality.
- 11.4 Accordingly, it is recommended that the scheme should be deferred and delegated to the Chief Planning Officer for approval subject to the conditions specified in Appendix A (including any amendment to the same or addition of further conditions as the Chief Planning Officer deems appropriate) and the completion of a Section 106 agreement.

**Background Papers:** Application file 22/04852/FU, two letters of objection, two objections from Leeds Civic Trust, Appendix A – Conditions and Appendix B - Operator Information

## APPENDIX A – Draft Conditions

- 1 The development hereby permitted shall be begun before the expiration of three years from the date of this permission.  
Imposed pursuant to the provisions of Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.
- 2 The development hereby permitted shall be carried out in accordance with the approved plans listed in the Plans and Specifications above.  
For the avoidance of doubt and in the interests of proper planning.
- 3 The development shall be carried out in accordance with the details shown on the submitted plan, "Flood Risk Assessment' 600512 (rev V02) prepared by Hexa, dated 4/07/2022", unless otherwise agreed in writing with the Local Planning Authority. (In the interest of satisfactory and sustainable drainage)
- 4 If remediation is unable to proceed in accordance with the approved Remediation Strategy, or where significant unexpected contamination is encountered, or where soil or soil forming material is being imported to site, the Local Planning Authority shall be notified in writing immediately and operations on the affected part of the site shall cease. The affected part of the site shall be agreed with the Local Planning Authority in writing. An amended or new Remediation Strategy and/or Soil Importation Strategy shall be submitted to and approved in writing by the Local Planning Authority prior to any further remediation works which shall thereafter be carried out in accordance with the revised approved Strategy. Prior to the site being brought into use, where significant unexpected contamination is not encountered, the Local Planning Authority shall be notified in writing of such.

It is strongly recommended that all reports are prepared and approved by a suitably qualified and competent person.

To ensure that any necessary remediation works are identified to make the site 'suitable for use' with respect to land contamination

- 5 The approved Phase I Desk Study report indicates that a Phase II Site Investigation is necessary, and therefore development (excluding demolition) shall not commence until a Phase II Site Investigation Report has been submitted to and approved in writing by the Local Planning Authority. Where remediation measures are shown to be necessary in the Phase II Report and/or where soil or soil forming material is being imported to site, development (excluding demolition) shall not commence until a Remediation Strategy demonstrating how the site will be made suitable for the intended use has been submitted to, and approved in writing by, the Local Planning Authority. The Remediation Strategy shall include a programme for all works and for the provision of Verification Reports.

It is strongly recommended that all reports are prepared and approved by a suitably qualified and competent person.

To ensure that the presence of contamination is identified, risks assessed and proposed remediation works are agreed in order to make the site 'suitable for use' with respect to land contamination.

- 6 Remediation works shall be carried out in accordance with the approved Remediation Strategy. On completion of those works, the Verification Report(s) shall be submitted to the Local Planning Authority in accordance with the approved programme. The site or phase of a site shall not be brought into use until such time as all verification information has been approved in writing by the Local Planning Authority.

It is strongly recommended that all reports are prepared and approved by a suitably qualified and competent person.

To ensure that the remediation works are fully implemented as agreed and the site has been demonstrated to be 'suitable for use' with respect to land contamination."

- 7 Prior to the commencement of the above ground building structure an updated Sustainability Statement shall be submitted to and approved in writing by the Local Planning Authority, which will include a detailed scheme comprising:
- a. Recycled material content plan
  - b. Site Waste Management Plan
  - c. Energy plan showing the amount of on-site energy produced by the selected Low and Zero Carbon (LZC) technologies and that it produces a minimum of 10% of total demand for each building, including product specifications
  - d. finalised location of future district heating connection point or proposals of alternative methods to demonstrate compliance with Core Strategy Policy EN1The development shall be carried out in accordance with the approved details

Within 6 months of first occupation of the development, a post construction review statement including evidence of implementation of the low water usage target 110 litres/person/day, shall be submitted to and approved in writing by the Local Planning Authority. The development and buildings comprised therein shall be maintained and any repairs shall be carried out all in accordance with the approved detailed scheme and post completion review statement and certification.

In the interests of ensuring the development meets the requirements of the adopted energy policies within the Core Strategy.

- 8 Within 6 months of first occupation of the development, a post construction review statement including as-built evidence of compliance with Core Strategy Policies EN1 and EN2, shall be submitted to and approved in writing by the Local Planning Authority. The development and buildings comprised therein shall be maintained and any repairs shall be carried out all in accordance with the approved detailed scheme and post completion review statement and certification."

In the interests of ensuring the development meets the requirements of the adopted energy policies within the Core Strategy.

- 9 No works to or removal of trees or shrubs with bird-nesting potential shall take place between 1st March and 31st August inclusive, unless a competent ecologist has undertaken a careful, detailed check of vegetation for active birds nests immediately before (within 24 hours) the works commence and provided written confirmation that no birds will be harmed and/or that there are appropriate measures in place to protect nesting bird interest on site. Any such written confirmation should be submitted to the LPA within 3 days of such works commencing.

To protect nesting birds in vegetation and built structures in accordance with the Wildlife & Countryside Act 1981 (as amended) and BS 42020:2013.

- 10 (Pending Biodiversity Net Gain resolution, may be removed) Prior to the completion of the above ground building structure, a Plan shall be submitted to and approved in writing by the LPA of: integral bat roosting and bird nesting features (for species such as House Sparrow and Swift) within buildings. The agreed Plan shall show the number, specification of the bird nesting and bat roosting features and where they will be located, together with a timetable for implementation and commitment to being installed under the instruction of an appropriately qualified bat consultant. All approved features shall be installed prior to first occupation of the dwelling on which they are located and retained thereafter.

To maintain and enhance biodiversity in accordance with Core Strategy Policy G9, NPPF, and BS 42020:2013.

- 11 No above ground works shall be commenced until a ventilation and overheating mitigation scheme in accordance with Noise Report C10677/NIA/1.0 for rooms where windows need to be kept closed to prevent excessive noise levels shall be submitted to and approved in writing by the Local Planning Authority. The ventilation and overheating mitigation scheme shall include the following information:

- Identify which rooms referenced back to the approved Noise Assessment require specific overheating mitigation
- The acoustic specification of the proposed ventilation system demonstrating that when operated it will not cause indoor noise target levels to be exceeded
- The ventilation scheme must demonstrate that the air intake is located away from the sources of noise and/or poor air quality.

All works which form part of the approved scheme for each apartment shall be completed prior to occupation of the aforementioned apartment and retained thereafter.

The combined noise from any fixed mechanical services and external plant and equipment shall be effectively controlled so that the combined rating level of noise from all such equipment does not exceed the background sound level at any time. Rating level and background sound level are as defined in BS 4142:2014+A1:2019.

- 12 Prior to occupation, a post completion sound test including method statement detailing sampling locations to confirm compliance with specified criterion shall be submitted for approval. Testing shall only commence following approval of the method statement. Following approval of the method statement and subsequent testing process, in the event that sound levels exceed the specified limits, the applicant shall undertake corrective action and re-test. Once compliance can be demonstrated the results shall be resubmitted for approval.

The specified criteria are: noise rating BS8233 (35db/30db day/night) in bedroom between 23.00 and 07.00

In the interests of residential amenity

- 13 Commercial deliveries to and from the premises including loading and unloading and refuse collection, shall be restricted to 08.00 to 20.00 hours Monday to Saturday and 09.00 to 18.00 hours on Sundays and Bank Holidays.



In the interests of amenity.

- 14 a) prior to works concerning the installation of landscaping, no landscaping works shall take place until full details of the load bearing cell type rooting zone using proprietary structures has been submitted and approved in writing by the local planning authority. Details shall be fully in accordance with LCC guidance on urban tree planting (available on Landscape Planning website).

Details shall include:

- o proprietary soil cell structures to support paving over extended sub-surface rooting areas.
- o Soil cell volume /soil volume calculations.
- o specification of topsoil including additives and conditioners. Tree grilles and guards and means of anchoring root balls. Built-in Root Irrigation Pipe system with end cap and aeration system.
- o Passive and / or active irrigation including directed use of grey water / roof water or surface water infiltration to benefit planted areas. Details of distribution system and controls.
- o Tree grill details.
- o drainage system for tree pits.
- o Where applicable details of protection measures for statutory utilities and drainage.
- o Works shall then be carried out in accordance with the approved details.
- o Confirmation of Manufacturer supervision on site (free service).

b) To ensure full compliance, a brief report on the installation of the rooting zone system, including supporting photographic evidence, shall be submitted to the Local Planning Authority (LPA) when the works are still "open" to allow LPA inspection prior to any finish surfacing works. Seven days written notice shall be given to the Local Planning Authority that the rooting zone structures are in place to allow inspection and approval of them as installed. Confirmation is required that the installation has been overseen by the manufacturer of the system.

NOTE-this item cannot be discharged until post inspection approval is confirmed.

c) AA three-year irrigation programme for the trees (in accordance with BS 8545-2014 Trees from Nursery to Independence) shall be submitted to the Local Planning Authority (LPA) for approval in writing. Confirmation of irrigation compliance shall be submitted to the LPA on a quarterly basis for the full three-year programme period.

To ensure the provision of amenity afforded by appropriate landscape design and its cultural requirements are integrated into the development scheme.

- 15 If within a period of five years from the date of the planting of any tree/hedge/shrub that tree/hedge/shrub, or any replacement, is removed, uprooted or destroyed or dies, or becomes, in the opinion of the Local Planning Authority, seriously damaged or defective, another tree/hedge/shrub of the same species and size as that originally planted shall be planted in the same location as soon as reasonably possible and no later than the first available planting season, unless otherwise agreed in writing by the Local Planning Authority.

To ensure the provision of amenity afforded by appropriate landscape design.

- 16 Hard and soft landscaping works shall not commence until full details of both hard and soft landscape works, including an implementation programme and any temporary

treatments required have been submitted to and approved in writing by the Local Planning Authority.

Hard landscape works shall include:

- a. proposed finished levels and/or contours.
- b. boundary details and means of enclosure.
- c. other vehicle and pedestrian access and circulation areas.
- d. hard surfacing areas.
- e. Lighting.
- f. CCTV and access controls
- g. proposed and existing functional services above and below ground (e.g. drainage, power cables, communication cables, pipelines etc., indicating lines, manholes, supports etc.).
- h. access controls and site security measures.

Soft landscape works shall include.

- i. planting plans.
- j. written specifications (including soil depths and quality to BS 3882:2015, cultivation and other operations associated with plant establishment).
- k. schedules of plants noting species, planting sizes and proposed numbers/densities.
- l. details of tree pits and root cells.
- m. details of green roofing
- n. A scheme for management and maintenance of the publicly accessible areas
- o. long term landscape management plan.
- p. Finalised locations of external cycle parking.
- q. tree protection measures for existing trees and a methodology for their retention

All hard and soft landscaping works shall be carried out in accordance with the approved details, approved implementation programme and British Standard BS 4428:1989 Code of Practice for General Landscape Operations. The developer shall complete the approved landscaping works and confirm this in writing to the Local Planning Authority prior to the date agreed in the implementation programme.

In the interests of amenity, highway and public safety and to ensure the provision of amenity afforded by appropriate landscape design

- 17 No works shall commence until a Statement of Construction Practice has been submitted to and approved in writing by the Local Planning Authority. The Statement of Construction Practice shall include full details of:
- a) the methods to be employed to prevent mud, grit and dirt being carried onto the public highway from the development hereby approved;
  - b) the routes to be used for constructions vehicles;
  - c) measures to control the emissions of dust and dirt during construction;
  - d) measures to mitigate the impact of construction on other parts of the development including any trees proposed to be retained:
  - e) location of site compound and plant equipment/storage; and
  - f) how this Statement of Construction Practice will be made publicly available by the developer.

The approved details shall be implemented at the commencement of work on site, and shall thereafter be retained and employed until completion of works on site. The Statement of Construction Practice shall be made publicly available for the lifetime of the construction phase of the development in accordance with the approved method of publicity.

- 18 Prior to any construction works a Construction Traffic Management Plan shall be submitted to and agreed in writing by the Local Authority. It shall include but, not be limited to, details regarding:
- o Hours of operation.
  - o The number of daily HGV movements via the SRN.
  - o Delivery scheduling that should, wherever practically possible, ensure deliveries to and from site via the SRN are not undertaken during peak hours.
  - o HGV holding areas, that should not be located on or within close proximity of the SRN.
  - o Wheel Washing facilities.

As the carrying out of the development could result in significant harm to the amenities of local residents and/or highway safety and accordingly details of construction practice is required to be agreed prior to the commencement of works in order to protect such interests.

- 19 Development shall not be occupied until all areas shown on the approved plans to be used by vehicles, including roads, footpaths, cycle tracks, loading and servicing areas and vehicle parking space have been fully laid out, surfaced and drained such that loose materials and surface water does not discharge or transfer onto the highway. These areas shall not be used for any other purpose thereafter.

To ensure the free and safe use of the highway.

- 20 Notwithstanding the approved details, the building shall not be occupied until full details of cycle parking and facilities have been submitted to and approved in writing by the Local Planning Authority. The approved cycle parking and facilities shall be provided prior to first occupation of the building and retained thereafter for the lifetime of the development.

In the interests of highway safety and promoting sustainable travel opportunities.

- 21 Development shall not be occupied until the Electric Vehicle Charging Point has been provided in accordance with a scheme that shall have been submitted to and approved in writing by the Local Planning Authority. The approved facilities shall thereafter be retained for the lifetime of the development.

In the interest of promoting low carbon transport.

- 22 No part of the development hereby permitted shall be occupied until details for the provision of bin stores (including siting, materials and means of enclosure) and (where applicable) storage of wastes and access for their collection has been submitted to and approved in writing by the Local Planning Authority. The approved measures shall be implemented in full before the use of that phase commences and shall be retained thereafter for the lifetime of the development.

To ensure adequate measures for the storage and collection of wastes are put in place. In the absence of appropriate measures residential amenity could be adversely affected.

- 23 Prior to the installation of any external facing material, full details including a sample panel of the relevant external facing materials, roofing and full details of glazing types for that building to be used shall be constructed on-site and approved in writing by the

Local Planning Authority. The external materials, roofing and glazing materials shall be constructed in strict accordance with the sample panel(s). The sample panel(s) shall not be demolished prior to the completion of the development, unless otherwise agreed in writing by the Local Planning Authority.

In the interests of visual amenity.

- 24 Prior to the construction of the following elements of the proposed building, full 1 to 20 scale working drawing details of the following for that phase shall be submitted to and approved in writing by the Local Planning Authority:
- a. soffit, roof line, eaves and any external plant area treatments.
  - b. junctions between materials.
  - c. each type of window bay proposed.
  - d. ground floor frontages.

Development shall then be undertaken in accordance with the approved details. In the interests of visual amenity.

- 25 Prior to the installation of the wind mitigation measures full visual and locational details of the proposed wind mitigation measures, method of affixment and control of delivery shall be submitted to and approved in writing by the Local Planning Authority. The approved measures shall be installed prior to occupation and maintained thereafter.

In the interests of creating a safe wind environment, adequate mitigation and public safety.

- 26 Prior to installation of the agreed wind mitigation measures a scheme for testing and assessing the effectiveness of the approved mitigation measures shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall incorporate details and timescales for the implementation of any further mitigation measures where these are found to be necessary by the testing exercise and shall be implemented in accordance with the details and timescales thereby approved. Removal of the required mitigation measures shall only be accepted following the submission of evidence and appropriate testing justifying removal, submitted to and approved in writing by the Local Planning Authority.

In the interests of creating a safe wind environment, adequate mitigation and public safety.

- 27 Prior to the installation of any extract ventilation system or externally mounted mechanical plant, details of such systems, including where relevant details of odour and smoke filtration for hot food uses, shall be submitted to and approved in writing by the Local Planning Authority. The equipment shall aim to achieve the criteria set out in Leeds City Council Planning Consultation Guidance 'Noise and Vibration' dated December 2019.

- 28 Any external extract ventilation system/air conditioning plant shall be installed and maintained in accordance with the approved details. The rating level of noise from any externally mounted plant or equipment is to be no higher than the existing representative background noise level (LA90) when measured at noise sensitive premises, with the measurements and assessment of calculation made in accordance with BS4142:2014.

In the interests of amenity.

27 Prior to first occupation, a sound insulation scheme related to any amenity space within the amenity areas of the building hereby approved to be used as a gym or spa shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall achieve internal residential noise levels of no higher than noise rating NR20 in bedrooms between 23.00 and 07.00, and NR25 in all habitable rooms between 07.00 and 23.00. Associated plant noise from the amenity spaces shall achieve a BS4142:2014 rating level of no higher than the background at the nearest noise sensitive receptors, including the character corrections for tonality, impulsivity and intermittency as appropriate.

In the interest of amenity.

28 Prior to first occupation, a security and building management plan shall be submitted to and approved in writing by the Local Planning Authority. The plan shall detail how tenant management between residential units will be addressed and what safety measures and procedures shall be implemented to ensure that tenants can co-habit safely. The development shall then be undertaken in accordance with the approved details

In the interests of public safety and residential amenity

29 Notwithstanding the details shown on the approved Proposed Site Layout, the parking area shall not be laid out or be brought into use until a revised Proposed Site Layout demonstrating the provision of disabled car parking has been submitted to and approved in writing by the Local Planning Authority, (in accordance with current British Standard BS8300 unless otherwise agreed in writing). The agreed layout shall be implemented prior to occupation and retained for the lifetime of the development.  
To ensure the provision of disabled parking.  
Development shall not commence until a survey of the condition of Brick Street and Brussels Street has been submitted to and approved in writing by the Local Planning Authority. Upon completion of the development (completion of the final approved building on the site) a further condition survey shall be carried out and submitted to the Local Planning Authority together with a schedule of remedial works to rectify damage to the highway identified between the two surveys. The approved mitigation works shall be fully implemented six months of the remedial works being agreed with the Local Planning Authority. In the event that a defect is identified during other routine inspections of the highway that is considered to be a danger to the public it must be immediately made safe and repaired within 24hours from the applicant being notified by the Local planning Authority.  
As traffic associated with the carrying out of the development may have a deleterious effect on the condition of the highway that could compromise the free and safe use of the highway.

30 Prior to occupation of the development, the off-site highway works as shown on plan 21034-p401c comprising re-aligned turning head, provision of street trees and footway improvements at Brick Street and Brussels Street shall be fully delivered.  
To ensure the free and safe use of the highway.

31 No development shall commence (excluding the demolition of existing structures and site clearance) until;

- a) a scheme of intrusive investigations has been carried out on site to establish the risks posed to the development by past coal mining activity; and
- b) any remediation works and/or mitigation measures to address land instability arising from coal mining legacy, as may be necessary, have been implemented on site in full in order to ensure that the site is made safe and stable for the development proposed.

The intrusive site investigations and remedial works shall be carried out in accordance with authoritative UK guidance.

In the interests of safety and coal mining legacy

32 Prior to the occupation of the development, or it being taken into beneficial use, a signed statement or declaration prepared by a suitably competent person confirming that the site is, or has been made, safe and stable for the approved development shall be submitted to the Local Planning Authority for approval in writing. This document shall confirm the methods and findings of the intrusive site investigations and the completion of any remedial works and/or mitigation necessary to address the risks posed by past coal mining activity.

In the interests of safety and coal mining legacy



### VERVLIFE

VervLife specialist operators in co-living and build to rent. We get involved with clients as the earliest stage of a project, advising on viability, feasibility, design and layout. With the aim to ensure that buildings are as efficiently designed and built as possible, putting resident experience at the forefront, but also ensuring that the operating costs are kept within an acceptable level. We then move through the different stages of the project, working on a leasing and marketing strategy, setting rental levels, mobilising the scheme and taking it into day to day operational management. We strive to improve the wellness, health and happiness of our residents whilst they are living with us, as well as enhancing the value of our clients investments. VervLife have a number of co-living and build to rent schemes under management

### GROUND FLOOR



### HOLDFORTH COURT – CO-LIVING

Co-living is a different way of renting. Allowing residents flexibility not only from the length of time they can chose to rent for, short or long term, but also because of the way the building in managed and the offerings available. Holdforth Court will have an onsite team and the scheme will provide a secure bike store, secure parcel storage, a laundry, co-working space, residents lounges, a gym, roof terraces and a shared kitchen on each floor. Rents are all inclusive of utility bills and broadband which gives residents the comfort of knowing their monthly expenditure will remain at a constant level with no unexpected costs.

Community - From earliest opportunity building a sense of community is critical for the success of the scheme. Group viewings will be undertaken so that potential residents can meet each other and get a feel for what the experience of living at Holdforth Court will be like. Community doesn't just mean within the four walls of Holdforth Court, but extends out into the wider community and partnering with local businesses to offer resident discounts and forming strong bonds with the local community

Engagement – Having regular engagement and communication with the residents is critical to create the community. By using our Residents app we will be able to promote residents events and share local community benefits and discounts. The app also allows residents to access their tenancy documents, home user guides and pay rent.



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Environment – Smart metering will allow for monitoring of energy use both within the apartments and also the amenity spaces. The apartments will be well constructed and insulated which will reduce energy use. The roof terraces will be designed to encourage biodiversity and encourage wildlife. Holdforth Court has minimal car parking spaces and a large number of bike racks which is environmentally friendly and allow residents to cycle around the City to and from work.

## COMMUNAL KITCHENS

The shared kitchens on each floor could be themed as seen in the examples from The Collective, Old Oak Co-living scheme as seen below. These kitchens allow for residents to have the opportunity to cook and eat together. Bringing different nationalities and ages of residents together socially.



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The below shows examples of how laundry facilities can be brought into communal spaces such as café space which will bring residents together.



The Collective – Old Oak – London



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The below shows examples of well equipped gym which allows residents the opportunity to exercise without needing to leave the building and is a good social space which is an important offering for health and wellbeing.



Blackhorse Mills – London



Duet – Manchester



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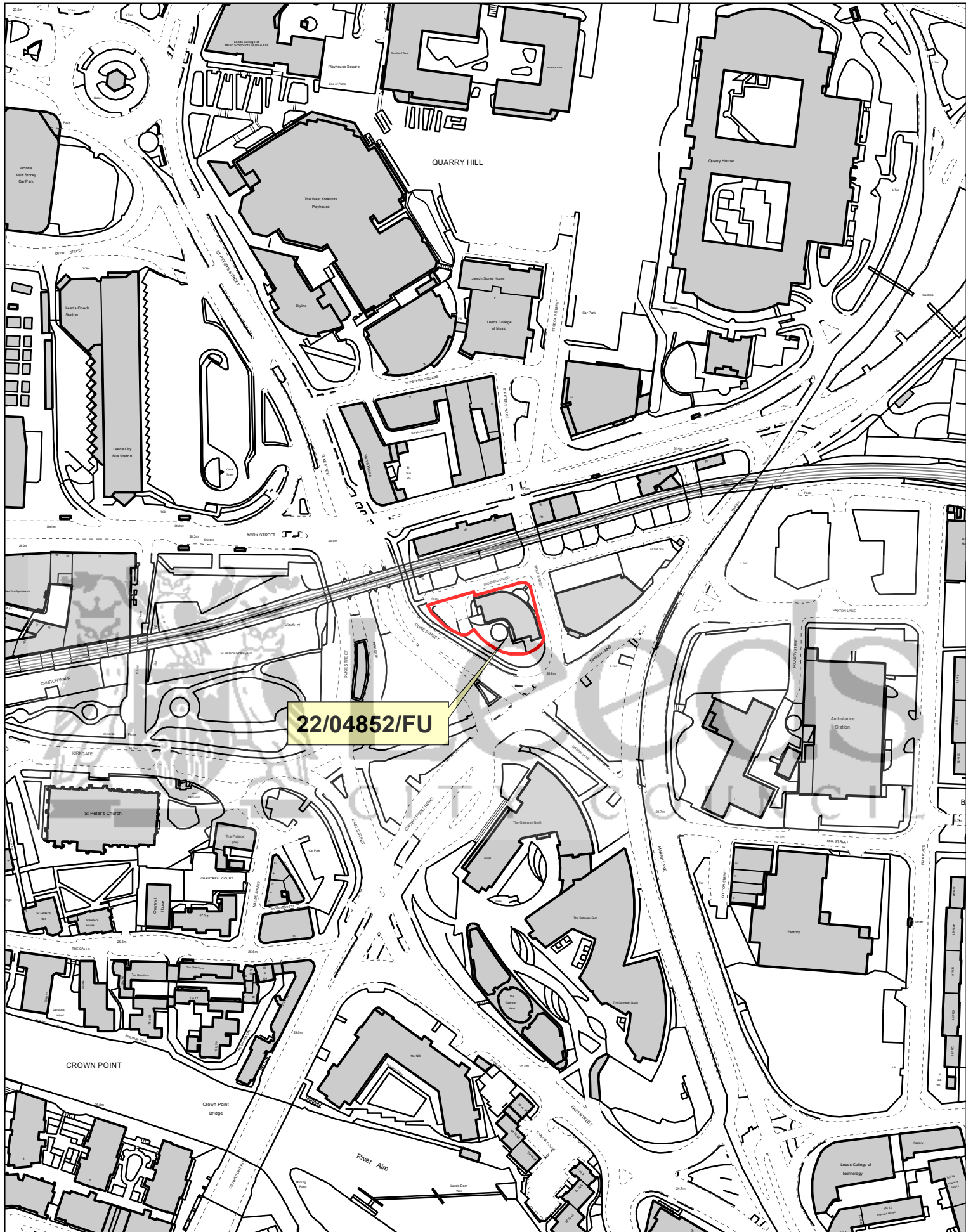
Well design Co Work space offers residents the opportunity to work from home, but in a social environment. These spaces can also double up to be utilised for residents events and social meet up.



Wembley Ark – Wembley, London



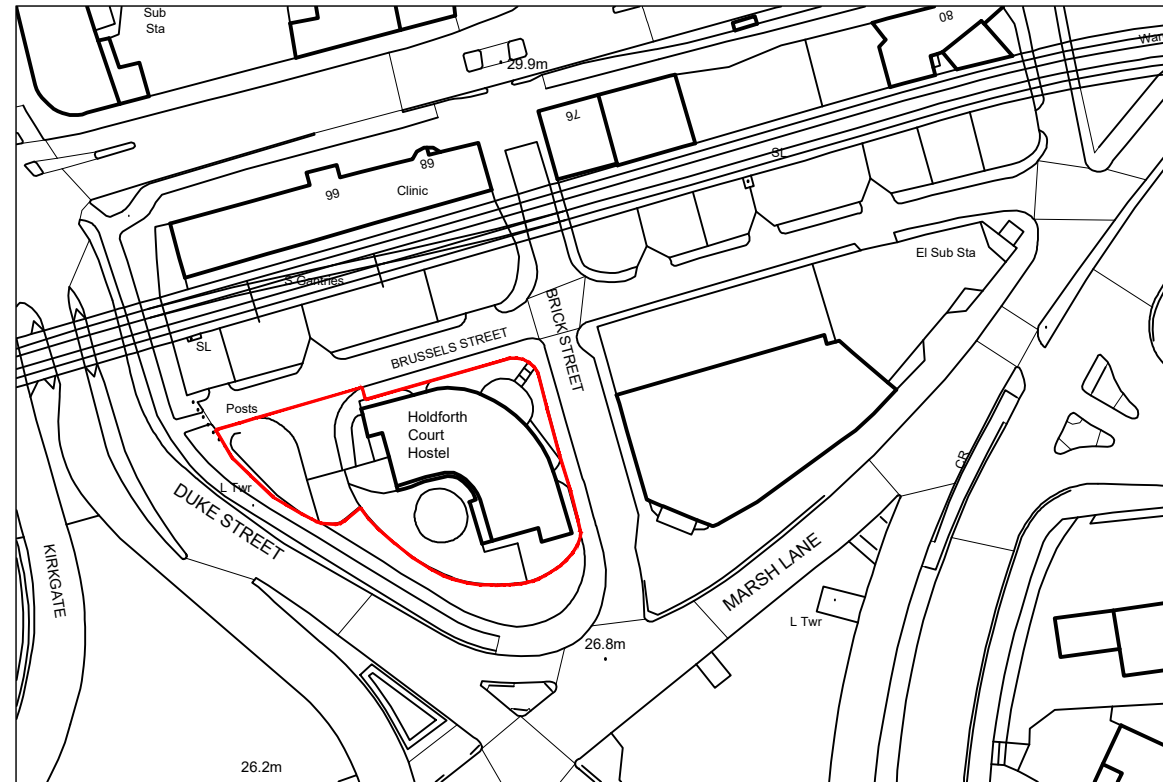
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22/04852/FU

# CITY PLANS PANEL

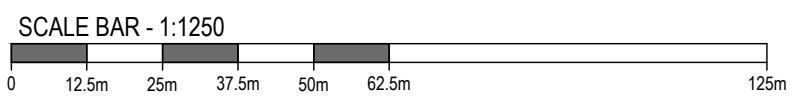
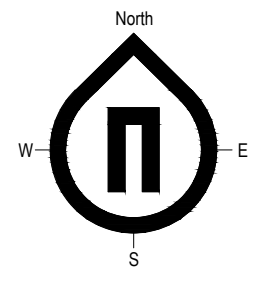




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**DRAWING STATUS/TYPE KEY**  
 F Feasibility    SK Sketch    L Landscape    C Construction  
 P Planning    M Marketing    S Survey    AB As Built  
 T Tender    TNT Tenant    OS Ordnance Survey

**SURVEY NOTE**  
 The original building survey was undertaken by a third party. NW Architects accepts no responsibility for any errors, omissions or discrepancies.



REV	DESCRIPTION	DRAWN	DATE	CHECKED	DATE
<b>REVISIONS</b>					

**NW-ARCHITECTS**  
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**PROJECT**  
 Marsh Lane / Holdforth Court  
 Leeds

**DRAWING TITLE**  
 Site Location Plan  
 Existing

Scale	Drawn By	Date Drawn
1:1250 @ A3	RS	02-08-22
	Checked By	Date Checked
	NW	02-08-22

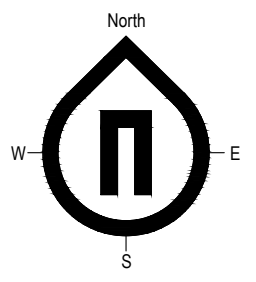
Drawing No.	Revision
21034-S101	-



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**DRAWING STATUS/TYPE KEY**  
 F Feasibility SK Sketch L Landscape C Construction  
 P Planning M Marketing S Survey AB As Built  
 T Tender TNT Tenant OS Ordnance Survey



REV	DESCRIPTION	DRAWN	DATE	CHECKED	DATE
C	SUB STATION, DIS PARKING, BIKE STORE AMENDED	AP	15-12-23	NW	12-12-23
B	EXTERNAL DOOR ADDED TO THE PLANT	AP	20-09-23	NW	29-09-23
A	ISSUED FOR PLANNING	AP	13-09-23	NW	13-09-23

**REVISIONS**

**NW-ARCHITECTS**  
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CLIENT

PROJECT  
 Holdforth Court  
 Leeds

DRAWING TITLE  
 Site Plan  
 Proposed

Scale	Drawn By	Date Drawn
1:500 @ A3	AP	24-08-23
	Checked By	Date Checked
	NW	24-08-23

Drawing No. 21034-P400  
 Revision C

**SITE PLAN PROPOSED**  
 SCALE BAR - 1:500  
 0 5m 10m 15m 20m 25m 50m

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